



August 24, 2022

Special Meeting | 7:00 p.m.

Troutdale Police Community Center – Kellogg Room
234 SE Kendall Ct, Troutdale, OR 97060

Agenda

1. Call to Order, Roll Call, & Pledge of Allegiance
2. Public Comment on Non-Agenda Items
3. Public Hearing
 - a. Public Hearing Procedure
 - b. Case File LU-0013-2022 – Home Forward
Type III Application – Planning Commission Decision
4. Public Hearing
 - a. Public Hearing Procedure
 - b. Case File LU-0014-2022 – Transportation System Plan Amendment
Type IV Application – Planning Commission Recommendation to City Council
5. Staff Communications
6. Commissioner Comments
7. Adjournment

Participation

The public may attend the meeting in person or via Zoom using the link below. Full Zoom details, including call-in information is available [online here](#).

This meeting location is accessible to persons with disabilities. A request for an interpreter for the hearing impaired or for other accommodations for persons with disabilities should be made at least 48 hours prior to the meeting to the City of Troutdale (comdev@troutdaleoregon.gov or 503-665-5175).

Next Meeting: Wednesday, September 14, 2022 at 7:00 p.m.



Staff Report

Report Date: August 18, 2022
Meeting Date: August 24, 2022

FILE NUMBER / NAME	LU-0013-2022 Home Forward		
APPLICATION TYPE	Type III Variance, Site Development Review, Lot Line Adjustment		
PROJECT APPLICANT	Taryn Wheeler, MWA Architects	PROPERTY OWNER	Multnomah County
PROJECT LOCATION	SW Kendall Ct & SW 2 nd St	TAX MAP / TAX LOT #	1N3E25CB -02501 / R649790500 1N3E25CB -02601 / R649791090
LAND USE MAP	Commercial (C)	LAND USE ACTIVITY	Vacant
ZONING DISTRICT	Downtown Mixed-Use (MU-1)	OVERLAY DISTRICT	Town Center, Slope District

PROPOSAL

The Applicant is seeking a Type III Variance, Site Development Review, and a Lot Line Adjustment on the subject property for a 94-unit affordable multi-family housing development in three, three-story buildings. The application was elevated to a Type III with the request of a variance to the off-street parking standards, the site development review is to ensure the design standards for multi-family housing is met and the lot line adjustment will remove the existing property line between the two tax lots.

VICINITY MAP



Location of Street View Perspective

STREET VIEW (Google 2018)



View of Property in Question looking southwest from SW Kendall Ct.

APPLICATION HISTORY

An introduction meeting for the project was held on August 23, 2021, followed by five additional meetings between October 2021 and January 2022 to discuss various items. There were two formal pre-application meetings for this application in February 2022, followed by an additional meeting in March 2022. The application was submitted on June 15, 2022 and was deemed complete on July 20, 2022.

PROCEDURE

This application is undergoing a Type III quasi-judicial procedure. [TDC 2.060] This procedure requires a Public Hearing and Planning Commission review in order to be adopted. Planning Commission is the decision-making body for this application and may approve, approve with conditions, or deny this application. Nearby property owners, relevant review entities, and other stakeholders have been notified accordingly. [TDC 2.075 - 2.090]

APPLICABLE CRITERIA

- *Troutdale Development Code (TDC): Ch. 1* Introductory Provisions; **Ch. 2** Procedures for Decision Making; **Sec. 3.211** Downtown Mixed-Use (MU-1); **Sec 4.300** Vegetation Corridor and Slope District (VECO); **Sec 4.600** Town Center; **Sec. 5.600** Erosion Control and Water Quality Standards; **Sec 5.700** Stormwater Management; **Sec. 5.1000** Public Improvements; **Sec 6.1300** Variance; **Ch. 8** Site Orientation & Design; **Ch. 9** Off-Street Parking and Loading; **Ch. 17** General Provisions; **Appendix A** Design Standards for Central Business District
- *Troutdale Municipal Code: 8.26* Outdoor Lighting
- *City of Troutdale Comprehensive Land Use Plan*
- *Town Center Plan 2020-2040*
- *Construction Standards for Public Works Facilities*
- Building and Fire Codes

REVIEW ENTITY COMMENTS

Listed below are review entities who received the Notice. If the entity provided comments, they can be found in a corresponding attachment.

Review Entity	Comments	Review Entity	Comments
Planning	Attachment 1	Mid-County Lighting District	Attachment 4
Building	Attachment 2	Gresham Fire & Emergency Services	Attachment 5
Public Works	Attachment 3	Mult. County Transportation	Attachment 6

OTHER COMMENTS

As of the date of this Staff Report, we have received six (6) comments from notified property owners.

Name	Comments	Name	Comments
Bill Wood	Attachment 7	James M. Gurtisen	Attachment 8
Denise Wood	Attachment 9	Paul Peterson	Attachment 10
Zelma Sutherland	Attachment 11		

RECOMMENDATION

For this application, Staff recommends Planning Commission vote for *approval with conditions*.

Staff has prepared a draft *Findings of Fact and Final Order* document, outlining how the decision criteria for this application were satisfied, along with proposed conditions of approval as outlined by the review entities. Any subsequent approvals from the City shall not be issued until all conditions listed in the attachments are adequately addressed as determined by the appropriate review entity. Planning Commission reserves the right to amend the draft and proposed conditions unless other governing documents or agreements state otherwise.

PROPOSED CONDITIONS OF APPROVAL

City of Troutdale Planning Division

1. Applicant shall be required to satisfy comments made by review entities, included in the attached Findings of Fact, or shall work with Staff to reach an agreement between the parties.
2. The approval of case file LU-0013-2022 shall expire automatically two (2) years from the effective date of the decision unless the approval is enacted either through construction, establishment of use, or recordation of plat or survey within the specified time period. The effective date of the decision shall be the date of the Notice of Decision, unless such decision is appealed.
3. The applicant shall resubmit the lot line adjustment record of survey prior to recordation with the County, which shall include all existing and proposed lot sizes, easements and to reflect the vacation of SW 2nd St.
4. The applicant shall demonstrate in their building plans that the parking landscaping standards within TDC 9.045 are met, and that the amount and dimensional requirements for bicycle parking are met per TDC 9.080.
5. In their building permit submittal, the Applicant shall demonstrate that the applicable architectural requirements of TDC 8.150.

City of Troutdale Building Division

1. Following Land Use approval, the applicant shall submit for a building permit as well as any applicable trade permits through the Building Division CSS portal (<https://troutdaleor-energovpub.tylerhost.net/apps/selfservice#/home>) as per the above provided comments.

City of Troutdale Public Works

1. The applicant shall pursue and complete the process for the partial vacation of SE 2nd Street as depicted on the applicant’s preliminary plans prior to receiving building permits for the site development.

2. The applicant shall submit with the building permit application an analysis by an engineer licensed in the State of Oregon assessing the adequacy of capacity of the sewer main in Kendall to its intersection with the main in Historic Columbia River Highway, considering the discharges of the proposed development as well as estimated discharges from other users of that subject main.
3. The applicant shall provide a Stormwater Management Report with the building permit applications that summarizes the stormwater design calculations and infiltration capacity of the soils, addresses the stormwater hierarchy, and addresses Source Control requirements, among other typical and customary elements of the SMR.
4. Applicant shall provide with the building permit application a supplement to the Trip Generation and Distribution Memorandum addressing trip distribution to the intersection of 4th and Buxton, daily and peak hours.
5. Applicant shall construct any/all remaining half-street improvements on SW 4th Street along the property frontage.
6. Applicant shall dedicate an easement to the City from the existing stub of SE 2nd Street to SW 257th Avenue for the future 2nd Street multi-use path, of sufficient width to accommodate the planned path and any necessary slopes.
7. The applicant shall dedicate an access easement to the City, for City operations, through the parking/drive aisle between SW Kendall Avenue and SW 4th Street, prior to receiving a Certificate of Occupancy.
8. The applicant shall construct sidewalk ramps meeting current ADA/PROWAG standards at the intersection of SW 2nd and Kendall with the sidewalk infill along Kendall. Design of the northern ramp must be coordinated with the design of the opposing ramp that will be constructed by another developer.
9. The applicant shall dedicate 8' wide utility easements along all property lines that abut City rights-of-way, excepting that where the setback is less than 8' the easement may be reduced to the width of the setback.
10. If the estimated cost of the public improvements is \$25,000 or more, the applicant shall complete the public improvements and receive a Certificate of Completion from Public Works prior to receiving building permits for the onsite buildings; unless the applicant requests and is granted a waiver as provided in TDC Chapter 7.

Gresham Fire & Emergency Services

1. Provide fire flow per Oregon Fire Code Appendix B. Fire flow for multifamily buildings varies based on construction type and square footage. OFC App B Table B105.1.
2. Temporary addresses of 6" shall be provided at EACH construction entrance prior to ANY construction materials arriving on site. Prior to building finals, a 6" permanent address placard will be required per the Gresham Fire Addressing Policy. OFC 505 & 1401
3. Required fire hydrants and access road shall be installed and approved PRIOR to any combustible construction material arriving on site. OFC 1412.1
4. Without knowing the building construction types or sizes, a PUBLIC fire hydrant is required to be within 225 feet of the main entrance driveway. The furthest point on each building shall be no more than 400 feet from a hydrant. Show on the building plans where the nearest existing and new hydrants are located. OFC Appendix C and 507
5. Each public or private fire hydrant used for fire flow for this property shall have a 5-inch Storz adapter with National Standard Threads installed on the 4 ½ -inch fire hydrant outlet. The adapter shall be constructed of high-strength aluminum alloy, have a Teflon coating on the seat and threads, and use a rubber gasket and two (2) set screws to secure it in place. The adapter shall be provided with an aluminum alloy pressure cap. The cap shall be attached to the hydrant barrel or Storz adapter with a cable to prevent theft of the cap. Adapter shall be Harrington HPHA50-45NHWCAP or equal approved by Gresham Fire.
6. The building is required to be provided with fire sprinklers throughout. OFC 903
7. A fire hydrant shall be within 50 feet of the fire sprinkler system "FDC". OFC Appendix C 102.2 & NFPA 13E
8. Fire hydrant locations shall be identified by the installation of reflective markers. The markers shall be BLUE. They shall be located adjacent and to the side of the centerline of the access roadway that the fire

- hydrant is located on. In case that there is no center line, then assume a centerline, and place the marker accordingly. OFC 508.5.4
9. All Fire Dept. Access Roads shall be drawn to scale and shown clearly on plans. The access roads shall be constructed and maintained prior to and during construction. The minimum width is 26' wide for locations where buildings are over 30' in height. Access roads in areas where fire hydrants are located are required to be a minimum width is 26' for a length of 20'. Clear height is a minimum 13' 6". OFC 1410, 503.2.1 & D103.1
 10. The required aerial fire access roads shall be located a minimum of 15 feet and a maximum of 30 feet from the building and shall be positioned parallel to one entire side of the building. This will be required to be approved by the fire code official. OFC App D-105.3
 11. Required Fire Dept. Access Roads on site shall be designed to support an apparatus weighing 75,000 lb. gross vehicle weight. Provide an engineer's letter stating the access road meets those requirements at time of building permit submittal. OFC, Appendix D, Section D102.1
 12. The turning radius for all emergency apparatus roads shall be: 28' inside and 48' outside radius. OFC 503.2.4
 13. No Parking Fire Lane signage or curb marking may be required. Fire access roads 20' – 26' wide require the marking on both sides. Indicate on the building permit plans. OFC D 103.6
 14. If a gate is installed on a fire access road, it must meet the requirements of the Gresham Fire Gate Policy. OFC 506.1
 15. A fire alarm system will be required. OFC 907, 903.4
 16. Prior to applying for a building permit provide a fire flow test and report. The fire flow report will verify that the correct fire flow is available and will be required to have been conducted within the last 12 months. OFC 507.3 & B-101.1
 17. A KNOX box will be required at the fire sprinkler riser room. If temporary construction gates are used, a KNOX exterior padlock will need to be installed for the duration of its use. These can be purchased at knoxbox.com/greshamfd OFC 506.1
 18. Class III standpipe required throughout buildings where the floor level of the highest story is located more than 30 ft above the lowest level of the fire department vehicle access. Shall be installed per NFPA 14. OFC 905.3.1
 19. The fire apparatus access shall extend to within 150 feet of all portions of the exterior walls of the first story of the building as measured by an approved route. OFC 503.1.1
 20. Fire apparatus access roadway grades shall not exceed 10%. Intersections and turnarounds shall be level (maximum 5%). Where fire apparatus access roadway grades exceed 10% but no more than 12%, buildings are required to have a fire sprinkler system installed. OFC 503.2.7, D103
 - 21.

Multnomah County Transportation Division

1. MCCR 7.000: The applicant must provide a Transportation Impact Study or Assessment that addresses the impact of the development on the level of service and capacity of the following intersections. This needs to be submitted to Multnomah County prior to Building Permit application:
 - i. Cherry Park Rd /Buxton Rd;
 - ii. East Historic Columbia River Highway/Buxton Rd
 - iii. Buxton/2nd
 - iv. Buxton/4th
2. MCCR 8.000 The applicant may be required to provide off-site improvements to mitigate impacts at intersections based on the information provided in the Transportation Impact Study.
3. The applicant is required to contact Mid-County Lighting District (chet.hagen@multco.us) to determine if streetlights are needed. If streetlights are required by the lighting district; this will need to be shown on plans at the time of Building permit application.
4. MCCR 26.000 Applicant must provide a stormwater certificate, and accompanying report with calculations, to Multnomah County (row.permits@multco.us) for review. Review required to ensure that stormwater does not impact County Transportation facilities. Fee for stormwater review \$200.

Mid-County Lighting District

1. Applicant shall be required to submit a Lighting Level Review and photometric calculations to assess the lighting levels for all public streets within their proposed project area.

GENERAL INQUIRIES

For more information, please contact Alex Lopez at alex.lopez@troutdaleoregon.gov or at (503) 674-7261.



DATE: August 17, 2022
FROM: Alex Lopez, Assistant Planner
TO: Taryn Wheeler, MWA Architects

SUBJECT: Type III – Variance, Site Development Review, Lot Line Adjustment – LU-0013-2022 Home Forward

The Planning Division has reviewed the Type III Variance, Site Development Review, Lot Line Adjustment LU-0013-2022 at SW Kendall Ct & SW 2nd St. This memo is to provide general comments found during the review of the submitted materials.

Troutdale Development Code (TDC)

Ch. 1 All items in this section shall apply unless otherwise mentioned herein.
(Introductory Provisions)

Ch. 2 The proposed application will follow a Type III procedure due to the request of a variance (>30%) to the off-street parking standards [TDC 6.1325].
(Procedures for Decision Making) This application will also include a Site Development Review [TDC 6.900] for the site improvements as well as a Lot Line Adjustment [TDC 7.180] to remove the existing lot line between the two tax lots, creating one single tax lot for the development.
 Notification procedures have been followed per the Type III standards for notice [TDC 2.060].
 A separate application for the street vacation of SW 2nd St will be applied for using the Type IV process for vacating a right-of-way [TDC 6.1200].

Sec. 3.211 The subject properties are zoned for *Downtown Mixed-Use (MU-1)*, which was previously named 'Central Business District (CBD)'.
(Downtown Mixed-Use; MU-1) Multi-family residential units are a permitted use within this zoning district [TDC 3.220].
 The subject properties are within the Town Center Overlay District (TC). The purpose of this district is to encourage the downtown Troutdale area to grow as a diverse and viable town center. The district allows for continued housing opportunities close to commercial activities.
 The proposed development would further fulfill the aforementioned mission of the TC overlay by creating new housing opportunities close to the commercial activities of downtown Troutdale.

Dimensional Standards (3.230.B):

- Minimum lot size: *(based off of the minimum lot width and minimum lot depth which would be 1,050sq.ft.)*
 - The subject property meets this requirement. The lot is approximately 156,014 SQ. FT in size.
- Minimum lot width: 15ft
 - The subject property meets this requirement. The lot has a width greater than 15ft.
- Minimum lot depth: 70ft [TDC 3.235.B.1]
 - The subject property meets this requirement. The lot has a depth greater than 70ft.
- Minimum lot frontage: 15ft
 - The subject property meets this requirement.

Setback Standards (3.230.B):

- Front yard setback: 10ft
 - The subject property meets this requirement.
- Side yard setback: *(along the property line that abuts the residential lots on SW 4th St)* 7 ½ ft [TDC 3.235.C.2.b.ii]
 - The subject property meets this requirement.
- Side yard setback: (along all other side lot lines) 5ft
 - The subject property meets this requirement.
- Rear yard setback: 10ft
 - The subject property meets this requirement.

While the height limit for this zoning district is 35ft., the three main buildings will be 38', 36'9" and 32' in height, with two buildings utilizing a provision for extra height as allowed by Senate Bill 8. SB 8 allows for 'affordable housing' properties to have up to 24ft of additional height.

Sec. 4.300
(Vegetation Corridor & Slope District; VECO)

The subject property contains a small area of slope greater than 25% along the property line abutting 225 SW 4th St and a small area along the property line abutting SW 4th St.

There is no VECO impact on the subject property.

The slope area will be addressed through the use of retaining walls along the areas where engineering has deemed it necessary to develop the site. Retaining walls will be used along the property line abutting 225 SW 4th St and the southern property line along the SW 4th St ROW.

All items in this section shall apply unless otherwise mentioned herein.

Sec. 4.600
(Town Center; TC)

The applicant has addressed the various applicable requirements for development as listed in the TC overlay in their narrative.

Page 110 of the adopted Town Center Plan lists 'High Density Residential' as one of the preferred future land uses for this site, citing the 'location in proximity next to the

police facility’ and ‘future development has limited visible impact to residential properties to the south’.

Staff finds that the applicant has met the requirements as necessary.

All items in this section shall apply unless otherwise mentioned herein.

Sec. 5.600
(Erosion Control & Water Quality Standards)

All items in this section shall apply unless otherwise mentioned herein.

Sec. 5.700
(Stormwater Management)

The applicant has provided a preliminary plan to manage stormwater. The City’s Public Works Department will review the submitted materials to ensure that their set of requirements for development regarding stormwater will be met.

All items in this section shall apply unless otherwise mentioned herein.

Sec. 5.1000
(Public Improvements)

Any required public improvements shall be completed prior to the issuance of building permits for the site. (TDC 5.1030.D)

Sec. 6.900
(Site Development Review)

The modification to the subject property will require a Type II Site Development Review, TDC 6.1915.B.

1. The application complies with all of the applicable provisions of the underlying zone and overlay zone(s), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards, or a variance or adjustment is granted.

FINDING: The application meets lot area, dimensions, density, building orientation, and lot coverage standards. There is no applicable floor area requirement. Architectural standards largely appear to be met through outright approval or through an administrative adjustment or during building plan review. All setback requirements have been met for this development. A provision within Senate Bill 8 allows the affordable-housing development to go over the maximum height limit of 35’. **As such, this criterion is met.**

2. The proposal includes required upgrades, if any, to existing development that does not comply with the applicable land use district standards, pursuant to Section 5.300 Nonconforming Uses.

FINDING: This proposal does not contain any required upgrades to any existing development that is non-conforming since the property is vacant and has never been developed. **As such, this criterion is not applicable.**

3. The proposal complies with all applicable site design and development standards of this Code, such as landscaping and parking.

FINDING: See ‘Findings of Fact and Final Order’ **As such, this criterion is conditionally met.**

4. The proposal meets all existing conditions of approval for the site or use, as required by prior land use decision(s) as applicable.

FINDING: There are no prior land use decisions for this property. As such, this criterion is not applicable.

Sec. 6.1200
(Vacation)

In a separate application, the applicant is requesting to vacate a 30ft wide section of the existing SW 2nd Street ROW.

The northern portion of Building A will encroach into the section of ROW that is to be vacated and will leave a 5' side yard building setback (as required per the MU-1 zoning district). The approval of this vacation (Type IV decision) will be added as a condition of approval of this application, since Building A will be located in the area that is to be vacated.

All other requirements for this application type will be addressed separately.

Sec. 6.1300
(Variance)

The Planning Commission may grant a variance under the Type III procedure if the request involves the expansion or reduction of a quantifiable provision in this Code by more than thirty percent (30%), or if the request is referred to the Planning Commission in accordance with Section 6.1335 of this Chapter. The variance shall be granted only if the Planning Commission determines that the criteria in Section 6.1315 of this Chapter are met

Please see the associated Findings of Fact and Final Order and Chapter 9 of this document for more details.

Ch 7.
(Land Divisions and
Lot Line
Adjustments)

The applicant is requesting a lot line adjustment to remove the existing lot line that separates the two existing tax lots.

The combination of the two existing tax lots will need to meet the submittal requirements per TDC 7.180.C.

The combination of the two tax lots into one tax lot will conform to the applicable zone and no boundary line will violate the applicable setbacks.

Ch. 8
(Site Orientation &
Design)

All items in this section shall apply unless otherwise mentioned herein.

Recreation or open space areas are not required for development within the area between HCRH and 2nd Street extended west to its intersection with SW 257th Drive from 257th Drive to the SE Sandy Street ROW. [TDC 8.150.B.7.b]

The applicant has indicated they wish to seek administrative adjustments to six design standards within Chapters 8 & Appendix A (Design Standards for Central Business District. [TDC 8.025]

- 8.150.B.4.a: *No wall of a residential building shall exceed a length of fifty (50) feet without a foundation offset of at least four (4) feet for a distance of at least sixteen (16) feet.* Staff finds that this administrative adjustment is reasonable and justified. The south elevation of Building B has a run 1' longer than 50ft without an offset. Due to the location of this elevation being closest to the residential home to the south, it has not been given any outdoor space or windows to break up the distance for added privacy.
- 8.150.B.4.f: *Roofline offsets shall be provided at intervals of forty (40) feet or less to create variety in the massing of structures and relieve the effect of a single, long roof. Roofline offsets shall be a minimum four (4) foot variation either*

vertically from the gutter line, or horizontally. Staff feels that this administrative adjustment is associated with the above-mentioned design standard.

- 8.150.B.4.d.i-iii: *The following window detail shall be incorporated into the building design:*
 - *i: Windows shall account for at least fifteen percent (15%) of any rear or front building elevation no matter what the building's orientation on the lot is.*
 - *ii: Windows shall account for at least ten percent (10%) of any side building elevation no matter what the building's orientation is.* Staff finds this administrative adjustment is reasonable and justified. The south elevation of Building B (a side building elevation) does not have any windows. Due to its location that faces closest to the neighbor to the south along SW 4th St, the windows have been oriented away from their view, therefore adding extra privacy for both the resident of the single-family home as well as the residents of the units in that building. The combination of topography, a lack of windows, and added vegetation will provide extra privacy for the development.
 - *iii: All windows shall have outer casings or frames.*
- 8.150.B.6.a-b: *Outdoor private space. Outdoor private space is required for each residential dwelling unit.*
 - *a. Each ground level dwelling unit shall have an attached accessible outdoor private space of not less than sixty (60) square feet in area. Individual outdoor areas for ground level units must be visually screened from each other by walls, fences, or vegetation screening that is at least six (6) feet high and totally sight-obscuring.* Staff finds that this administrative adjustment is reasonable and justified. The ground level dwellings will provide a sight-obscuring perforated metal pattern that is 18" shorter than the 60" requirement, but with added landscaping, the fence and landscaping can provide the same level of privacy for the ground level units.
 - *b. Each upper-level unit shall have an attached outdoor private space, such as a balcony, of not less than sixty (60) square feet in area. The area shall be enclosed, screened, or otherwise designed to provide privacy from adjacent units by walls, building offsets, or similar sight-obscuring screening.* Staff finds that this administrative adjustment is reasonable and justified. There are five (5) studio units that do not have an outdoor balcony due to their location either above the community room or on a façade that only has inset balconies. Adding bolt-on balconies to the 5 units above the community room would disrupt the cohesive design language for the building closest to the main entrance and most visible location of the development. The upper floor balconies are 10sq.ft. less than the 60sq.ft. requirement for outdoor space. Staff feels that the provided amount of outdoor space is adequate and appropriate for this development given that almost all units will have this outdoor space to utilize.

See Appendix A for the adjustment requested for A.109.C *Upper Floor Window Standards*.

All other items in this section shall apply unless otherwise mentioned herein.

<p>Sec. 8.060 (Landscaping and Screening)</p>	<p>The minimum area of a site to be retained in landscaping for a property within the MU-1 zoning district is 5% [TDC 8.060.A.1].</p> <p>The proposed development has retained 40.4% of the area in landscaping. The proposed development meets the required landscaping standards.</p> <p>The development will have additional landscaping around the residential properties on the southern property line with sight-obscuring plantings that will help limit the view of the development to only the top floor of the adjacent buildings, as required per TDC 8.060.</p>
<p>Ch. 9 (Off-Street Parking & Loading)</p>	<p>For multi-family dwelling units, the off-street parking requirement is 2 spaces per dwelling unit plus 1 space per three dwelling units for guest parking. [TDC 9.010]</p> <p>The potential 2nd Street Bridge over SW 257th Ave has been proposed to utilize an easement across a portion of the subject property that would connect onto the proposed to be partially vacated SW 2nd St ROW that currently exists.</p> <p>Staff will factor in the 11 on-street parking spaces for this application since previous multi-family residential applications with parking variance requests have also counted the on-street spaces towards the reduction of off-street required parking. [TDC 9.040.C]</p> <p>The applicant has provided 3 options for this site, one of which includes, at the northern panhandle, an additional 25 spaces, but would not accommodate for the proposed 2nd Street Bridge (via a City easement across that portion of the property), this is labeled as Option C.</p> <p><u>Required Off-Street Parking:</u></p> <p>2.33 spaces per DU (2.33 x 94 units) = 219 off-street parking spaces required.</p> <p>(Minus 11 spaces per the on-street reduction in TDC 9.040.C) = 208 off-street parking spaces required.</p> <p><u>Proposed Off-Street Parking:</u></p> <p>Option A/B:</p> <p>119 total off-street parking spaces provided [1.27 spaces per DU] 42.7% variance to the off-street parking space requirement</p> <p>Option C:</p> <p>144 total off-parking spaces provided [1.53 spaces per DU] 30.7% variance to the off-street parking space requirement</p> <p>The applicant has submitted for a variance to the off-street parking standards since both design options involve the reduction of a quantifiable provision in this Code by more than thirty percent (30%), the variance is elevated to a Type III, which results in the Planning Commission being the approval body for such application types. [TDC 6.1325].</p> <p>Applicant will need to provide additional short-term and long-term bicycle parking. Per TDC 9.080.A.1 & 9.080.A.2, the short-term bicycle parking shall be 5% of the</p>

required automobile spaces (5% of 219 is 11). The long-term bicycle parking shall be 3% of the required automobile spaces (3% of 219 is 7). This can be achieved through a condition of approval.

All items in this section shall apply unless otherwise mentioned herein.

Ch. 17 All items in this section shall apply unless otherwise mentioned herein.
(General Provisions)

Appendix A The applicant has requested an administrative adjustment to A.109.C *Upper Floor Window Standards* "Glass area dimensions shall not exceed 5'x7'".
(Design Standards for Central Business District) The upper floor windows of the residential units are shown to be 7'x6'-6" in dimension, exceeding the maximum size requirement per A.109.C. Additionally, the windows provide a 1" protrusion for window trim and the minimum required is at least 2" around the perimeter.

Staff finds that this administrative adjustment is reasonable and justified. The larger windows will provide the residents more daylight and views from their units. These windows will also help break up larger sections of walls without windows and are visually more appealing.

All other design requirements in Appendix A have either been met or are not applicable based off the subject property's location not adjacent to E. Historic Columbia River Hwy.

Troutdale Municipal Code (TMC)

TMC 8.26 The Mid-County Lighting District has provided comment regarding this application for the applicant to review in order to satisfy any requirements.
(Outdoor Lighting)

TMC 13.10.270 Any trees that are proposed to be removed shall be required to be shown on a tree removal plan with the application.
(Tree Removal)



DATE: August 8, 2022
FROM: Heather Jones, Building Inspector I (on behalf of the Building Official)
TO: Planning Division

SUBJECT: Building Division Comments – Home Forward (LU-0013-2022)

The Building Division has reviewed the application submittal for the **Home Forward (SW Kendall Ct. & SW 2nd St.)** and provides the following conditions and comments [based on the 2019 Oregon Structural Specialty Code (*Building Code*) and the provided land use application submittal materials]:

Comments

- This application is proposing a 94-unit affordable multi-family housing development with three buildings. The applicant will be seeking a Type III Variance, Site Development Review and a Lot Line Adjustment.
- Based on the proposed scope of work, the Building Division offers the below comments for each aspect of the project:
 - Type III Variance (parking standards): Building Division does not have comments.
 - Lot Line Adjustment: Building Division does not have comments.
 - Site Development Review: Building and trade permits will be required.
 - Phasing of any portion of this project must be done at the time of permit submittal.
 - Each building will be permitted separately for this project but there will only be one review (per phase as applicable).
 - If desired, the Building Division will facilitate a pre-submittal meeting to go over the permitting process with the development/design team prior to submitting for permits.
- Attached to these comments is a document titled *Commercial Building Design Criteria*. This guide is for use in navigating the *Building Code* requirements specific to our area (snow, seismic & wind loads). These criteria should be incorporated in your building permit plans and documents prior to submittal to the Building Division.
- Adopted building codes may be viewed online free of charge at the Oregon Building Codes Division website (<https://www.oregon.gov/bcd/codes-stand/Pages/adopted-codes.aspx>).

Conditions

- Following Land Use approval, the applicant shall submit for building permits as well as any applicable trade permits, through the Building Division CSS portal (<https://troutdaleor-energypub.tylerhost.net/Apps/SelfService#/home>) as per the above provided comments.

CONTACT INFORMATION:

Sean Blaire,
Building Official
City of Troutdale Building Division
(503)618-2794
sean.blaire@greshamoregon.gov

Heather Jones,
Building & Code Compliance Manager
City of Troutdale Building Division
(503)674-7262
heather.jones@troutdaleoregon.gov

ATTACHMENTS: Commercial Building Design Criteria

CITY OF GRESHAM

COMMERCIAL BUILDING/STRUCTURE DESIGN CRITERIA

CURRENT BUILDING CODES

- 2019 *Oregon Structural Specialty Code* (OSSC), based on 2018 *International Building Code* (IBC) and ASCE 7-16 *Minimum Design Loads and Associated Criteria for Buildings and Other Structures*
- 2019 *Oregon Zero Energy Ready Commercial Code*
Construction provisions:
Commercial: *ASHRAE Standard 90.1*
Multi-family: 2018 *International Energy Conservation Code*
- 2019 *Oregon Mechanical Specialty Code* (OMSC), based on 2018 *International Mechanical Code and International Fuel Gas Code*
- 2017 *Oregon Plumbing Specialty Code* (OPSC), based on 2015 *Uniform Plumbing Code*
- 2017 *Oregon Electrical Specialty Code* (OESC), based on 2017 *NFPA 70, National Electrical Code*

Building codes may be viewed online free of charge at the Oregon Building Codes Division website at <https://www.oregon.gov/bcd/codes-stand/Pages/adopted-codes.aspx>.

SNOW LOADS

Design snow loads for commercial buildings/structures shall be determined in accordance with Chapter 7 of ASCE 7-16, as modified by OSSC Section 1608. Ground snow load p_g shall be determined using the Structural Engineers Association of Oregon (SEAO) online tool at <http://snowload.seao.org/lookup.html>. Note that where the site elevation is above the “modeled elevation” reported by the online tool, the ground snow load value from the tool shall be increased per Table 7.2 of OSSC 1608.

Regardless of the value of ground snow load, or the roof snow load calculated per ASCE 7, the minimum design uniform roof snow load for any type of roof shall be 20 psf per OSSC 1608. A 5 psf rain-on-snow surcharge may apply per OSSC 1608; conservatively, a minimum 25 psf uniform roof snow load may be used. Additional load shall be applied for snow drift, sliding snow, etc. as required per ASCE 7 Chapter 7 and OSSC 1608, or based on site elevation, as discussed above.

SEISMIC LOADS

Every commercial building/structure and portion thereof, including non-structural components permanently attached to structures and their supports and attachments, shall be designed and constructed to resist the effects of earthquake motions in accordance with Chapters 11, 12, 13, 15, 17, and 18 of ASCE 7-16, as modified by OSSC Section 1613.

S_5 and S_1 , the short-period and 1-second period spectral response accelerations, illustrated in OSSC Figures 1613.2.1(1) and 1613.2.1(2), shall be determined using the Applied Technology Council (ATC) “Hazards by Location” online tool at <https://hazards.atcouncil.org>, with “ASCE 7-16” selected as the “reference document”.

Based on soil properties, a site shall be classified as *Site Class* A, B, C, D, E, or F in accordance with Chapter 20 of ASCE 7. Site Class D shall be used when soil properties are not known in sufficient detail (unless geotechnical data indicates Site Class E or F). Note that where Site Class D is used as the default due to insufficient knowledge of soil properties, the value of site coefficient F_a shall not be less than 1.2, per OSSC 1613.2.3.

All commercial buildings/structures located in the City of Gresham or areas where it has jurisdiction shall be assigned *Seismic Design Category* D at a minimum unless a lower category is justified by a site-specific study.

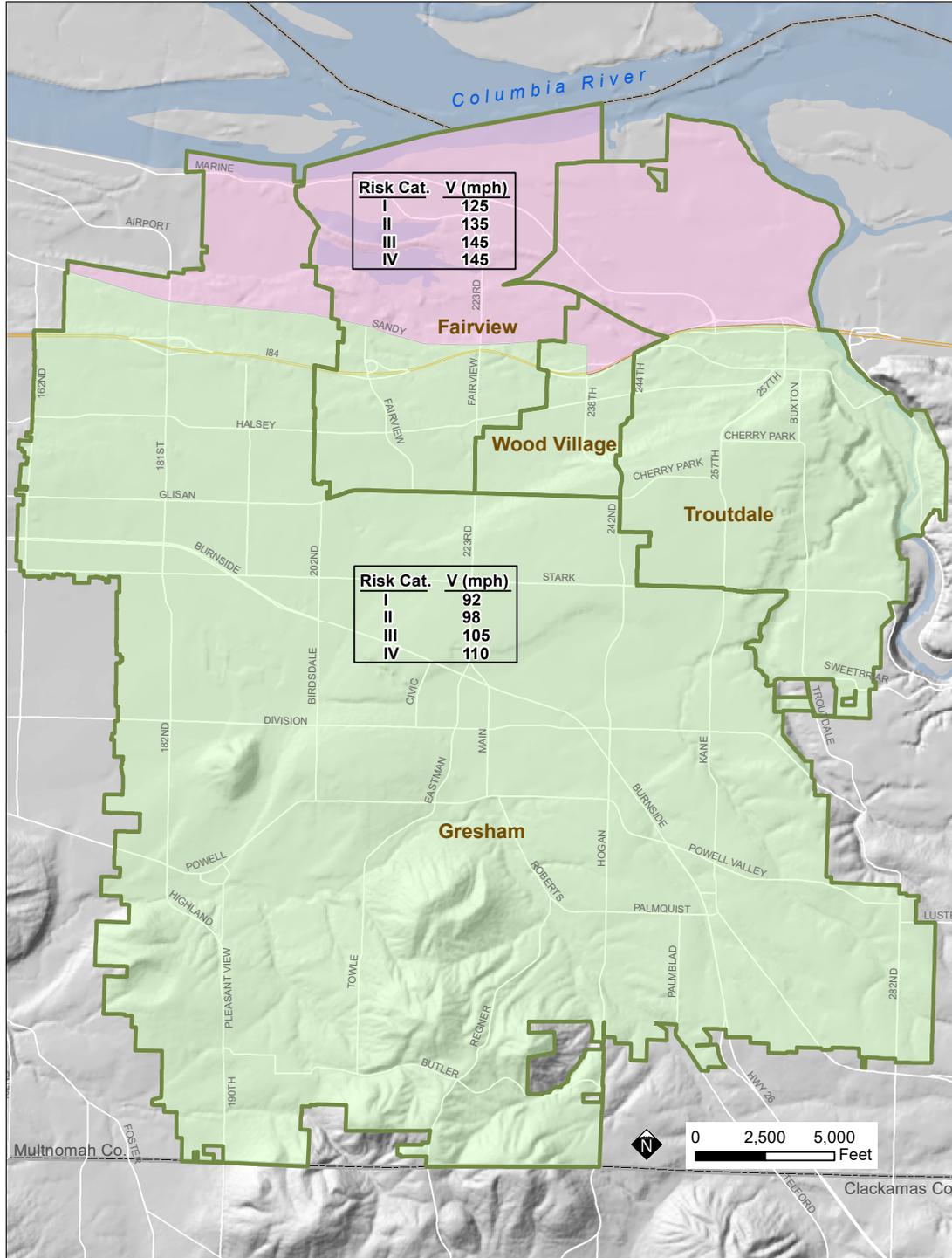
WIND LOADS

Design wind loads on every commercial building/structure or part thereof shall be determined in accordance with Chapters 26 to 30 of ASCE 7-16, as modified by OSSC Section 1609, and as modified by this document, including attached Figures 1 and 2.

For commercial buildings/structures located in the City of Gresham or areas where it has jurisdiction, the *basic design wind speed*, V , for each *risk category* shall be as shown in attached Figure 1, and the *exposure category* shall be as shown in attached Figure 2. (See OSSC Table 1604.5 for determination of risk category.)

**FIGURE
1**

Commercial building/structure basic design wind speed (*V*) by risk category

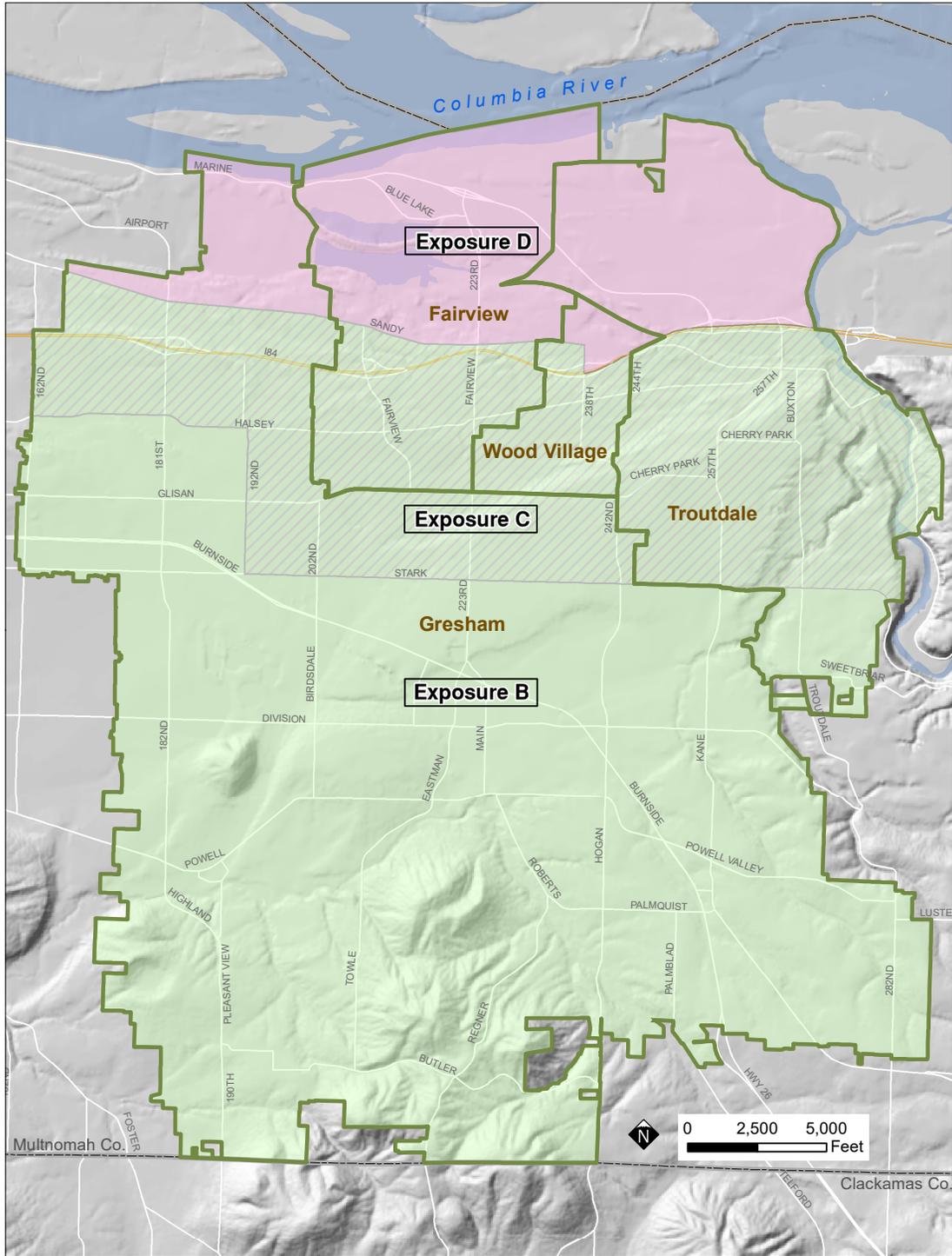


NOTE:

In areas of Multnomah County under the jurisdiction of the City of Gresham without a value shown on this map, the basic design wind speed shall be determined per OSSC Table 1609.3, including footnotes a and c, and OSSC Figure 1609.3, including footnotes a and b, with particular attention to determination of whether the site is in a mapped special wind region (use of online tool per Figure 1609.3 footnote a is required) and whether the site has full exposure to Columbia River Gorge winds (i.e. exposure category D). See Figure 2 of this document for determination of exposure category.

FIGURE
2

Commercial building/structure exposure category



NOTE:

1. The exposure category shown on this map shall be used unless justified otherwise by a rational analysis of the prevailing ground surface roughness determined from natural topography, vegetation, and surrounding construction for each wind direction, per ASCE 7-16 Section 26.7. Analysis shall be submitted to the City of Gresham for review and approval *prior to* submittal for permit (*strongly recommended to be submitted prior to the start of any structural design work*).
2. Unless justified otherwise per Note #1, in areas of Multnomah County under the jurisdiction of the City of Gresham without an exposure category shown on this map, exposure category C shall be used, except in areas of full exposure to Columbia River Gorge winds (typically adjacent to I-84 and/or U.S. Route 30) where exposure category D shall be used.



CITY OF TROUTDALE ENGINEERING DIVISION

MEMORANDUM

DATE: August 4, 2022
FROM: Travis Hultin, Interim PW Director; Chief Engineer
TO: Planning
CC: File
David Schaffer, Water & Streets Superintendent
Shawn Anderson, Wastewater Superintendent
Ryan Largura, Environmental Specialist

A handwritten signature in blue ink, appearing to be 'TH', is located to the right of the 'FROM' field.

**RE: Type III Variance, Site Development, and Lot Line Adjustment Review
File No. LU-0013-2022 (Home Forward)**

The Public Works Department has reviewed the Type III Variance, Site Development, and Lot Line Adjustment Review submittal for the Home Forward development. These comments are divided into two categories: general comments and proposed conditions. General comments are informational points to guide the applicant in the proper planning of public works infrastructure for this project, to inform the applicant of possible extraordinary issues and/or to provide the basis for findings. Proposed conditions are requirements that Public Works recommends be formally imposed on the developer in the final order. Note that references to the “City Standards” herein refer to the *Construction Standards for Public Works Facilities*.

General Comments/Findings

1. Any and all utility and transportation plans submitted with this application have been reviewed for the purpose of determining the feasibility of providing utility and transportation facilities for the project in accordance with City Standards. This land use approval does not constitute final approval of details, including but not limited to alignments, materials and points of access, connection or discharge, that are depicted or suggested in the application. The applicant is required to submit detailed construction drawings and/or plat drawings for the project, as applicable. The City of Troutdale Public Works Department will review plans, in detail, when they are submitted and approve, reject or require modifications to the plans or drawings based upon conformance with City Standards, the TDC and the professional engineering judgment of the Chief Engineer.
2. It is the opinion of the Public Works Department that the proposed multi-family affordable housing complex can be developed in accordance with the requirements of the Troutdale Development Code (as it pertains to Public Works requirements) and Construction Standards, provided it fully addresses the comments and conditions contained herein.
3. The lot consolidation does not pose any concern with regard to Public Works requirements. The applicant’s preliminary drawings appropriately indicate one service per lot (for the resulting lot)



CITY OF TROUTDALE ENGINEERING DIVISION

MEMORANDUM

for water and sewer. The lot consolidation will not result in any non-conforming City utility services.

4. The proposed partial street vacation is acceptable and preferable to Public Works as there is no reasonable potential for 2nd Street to extend further or to serve other users. As proposed, the vacation of 2nd Street is essential and required. See proposed Condition 1. The applicant has appropriately reflected that the existing SW 2nd Street stub will be demolished the sidewalk infilled by the applicant.
5. Adequate water service is available to the site and the applicant has indicated the installation of a new commercial water service assembly on the existing stub to the site from the main in SW Kendall.
6. Water system cross-connection (i.e. backflow) protection at the point of service (meter and/or fire vault) is required to protect public health. The type of backflow protection device required at the point of service is dependent on the fixtures, appliances and equipment connected to the onsite plumbing systems, point of use protections to be installed, and the nature of activities onsite. The applicant is encouraged to consult with a Public Works Cross Connection Specialist (503.674.3300) to determine the type of backflow protection device to be installed at the point of service, and reflect the required device on the plans. Prior to receiving a CofO (Certificate of Occupancy or equivalent), the applicant will be required to arrange a joint backflow protection site visit and consultation with the Public Works Cross Connection Specialist and the Plumbing Official. They will jointly tour the facility with the owner to review backflow protection devices at points of use and the point of service. If backflow protection upgrades are necessary, they will consult with and advise the owner on the most cost-effective means to achieve compliance. If any upgrades are needed, they must be completed prior to receiving a CofO. The applicant may elect to install a Reduced Pressure Backflow Protection Device at the point of service (and reflect it on the plans), in which case the above consultation is unnecessary and the site tour is not required. Upon installation, the developer must have the devices tested by a certified backflow assembly tester and register the device(s) with Public Works.
7. A public water main easement crosses the western side of the property and contains an active City water trunk main. The applicant has reflected this on the preliminary drawings and the drawings appropriately indicate no vertical structures within the easement.
8. The applicant will be required to decommission any known or discovered existing private water wells on the site in accordance with Oregon Water Resource Department requirements (if any).
9. Sanitary sewer service is available to the site and the applicant has indicated connection to the existing stub to the site from the main in SW Kendall. However, it is not known if the main in Kendall has adequate capacity to accommodate the discharge from the proposed development. The applicant is required to verify adequacy of the main's capacity to serve this development. If inadequate, the applicant will be required to construct any necessary upgrades. See proposed Condition 2.
10. The applicant will be required to decommission any existing underground septic or storage tanks present on the site prior to Certificate of Occupancy (if any).



CITY OF TROUTDALE ENGINEERING DIVISION

MEMORANDUM

11. The applicant will be required to submit an online Industrial User Wastewater Survey (IUWS) with building permit applications. Review of the IUWS may reveal additional sanitary sewer pretreatment requirements. The link to the online IUWS is available at <https://www.troutdaleoregon.gov/publicworks/page/public-works-forms-permits>. If needed, applicant shall contact Ryan Largura, Environmental Specialist in Public Works, for assistance in completing this survey.
12. The proposed project will result in more than 2,000 SF of new or redeveloped impervious area. Stormwater quality treatment is therefore required in accordance with the Portland Stormwater Management Manual (PSWMM). The applicant's preliminary plans indicate a stormwater treatment and disposal system that appears generally consistent with the design requirements of the PSWMM and is adequate to conclude that the development can meet the requirements. However, the applicant did not provide a Stormwater Management Report (SMR), and the stormwater treatment and disposal system they indicated does not meet category 1 of the PSWMM as it does not utilize surface vegetated facilities. The applicant must provide a Stormwater Management Report with the building permit applications that, among other requirements, addresses the stormwater hierarchy and provides rationale for not meeting category 1. See proposed Condition 3.
13. It appears from the preliminary plans that no offsite discharge of stormwater is proposed. Therefore, there are no concerns with capacity of existing public storm mains. If offsite discharge becomes necessary, the applicant may be required to provide a capacity analysis of the receiving public storm sewer main(s) and construct any necessary upgrades.
14. The site development area exceeds one acre. Therefore, the applicant will be required to obtain an NPDES 1200C permit through the City for construction stormwater management and erosion control prior to commencing any substantial ground disturbing activity.
15. The applicant has provided a Trip Generation and Distribution Evaluation Memorandum prepared by Dana Beckwith (Oregon PE 53044). As the title implies, this memo provides PM Peak and Average Daily Traffic projections, as well as forecasted distribution of those trips to adjacent roadways and intersections. It is not a full Traffic Impact Analysis, as it does not evaluate level-of-service/capacity impacts on the study intersections. The subject intersections are under the jurisdiction of Multnomah County, and Public Works recommends that the County require LOS/capacity analysis for the study intersections. The memorandum does not quantify distribution to, or impacts upon, SW 4th Street and its intersection with Buxton Road, which could be significant. 4th Street will provide a secondary access to the development, and might well be popular route. This could lead to substantial queuing on 4th during peak periods and should be addressed. See proposed condition 4.
1. The Troutdale Municipal Code and TDC specify that adjacent property owners are responsible for constructing the half-street improvements on their frontage. In this context, frontage means any property line that abuts the ROW line. See proposed Condition 5.
16. The applicant is seeking a variance to reduce the off-street parking requirement and has provided substantial documentation and analysis in support of that request. Off-street parking standards



CITY OF TROUTDALE ENGINEERING DIVISION

MEMORANDUM

are not the purview of Public Works and so Public Works does not provide recommendations for or against such variances, but can provide some comments to consider in evaluating the variance requested:

- a. The applicant has factored in assumed availability of on-street parking on City streets in their calculations and written testimony. On-street parking should not be considered in evaluating this variance, as it is specifically a variance to the off-street parking requirement, and the residents of this development will have no special reservation or right to use public on-street parking spaces any greater than any other member of the public. These on-street spaces are available to all and its notable that resident's usage of these spaces will consume existing parking supply for the greater downtown area, particularly during events. Therefore, the applicant's stated parking ratios should be adjusted to discount any on-street parking.
 - b. The site has traditionally been utilized for overflow parking during downtown events. While the applicant has no responsibility or obligation to make up for this lack of overflow parking, it does compound the need for available on-street public parking during those times and further emphasizes why the applicant should not be considering on-street public parking in its parking ratio analyses.
 - c. Public Works foresees substantial on-street parking on SW 4th Street, likely full utilization of on-street parking on 4th Street, despite the applicant's assumptions. 4th Street is 28' wide and the primary operational ingress/egress for Public Works Water and Streets operations. If on-street parking utilization on 4th Street is substantial, it will likely result in the necessity to designate No Parking on the south side of SW 4th Street throughout its length to enable smooth flow of two-way traffic.
17. The 2nd Street path and bridge are reflected in the City's Capital Improvement Plan. The applicant must therefore dedicate an easement from the existing 2nd Street stub to 257th Avenue to accommodate this path. See proposed Condition 6.
 18. The proposed accesses onto SW Kendall and SW 4th Street are acceptable. The applicant has provided site distance analysis indicating that these accesses can operate safely.
 19. This development will preclude any possibility of additional street connections to SW 4th Street. SW 4th Street is a primary operational and emergency response route for Public Works first responders and redundant access is sorely needed. To that end, the applicant shall allow emergency access through the parking/drive aisle between Kendall and 4th for the City. See proposed condition 7.
 20. Demolition of the 2nd Street stub and infill of the sidewalk may necessitate drainage facilities due to the existing topography of the intersection. If such facilities are necessary, the applicant will be required to install them.
 21. Where the sidewalk is infilled at the demolished 2nd Street stub, the applicant will be required to install sidewalks ramps meeting current PROWAG standards complementary to the opposing ramps at the intersection. Another current developer is required to construct the ramp at the NE



CITY OF TROUTDALE ENGINEERING DIVISION

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corner of 2nd/Kendall and the design of those complementary ramps must be coordinated. See proposed Condition 8.

22. SW Kendall Avenue is fully improved except as noted above. No half-street improvements are required on SW Kendall except as otherwise indicated herein.
23. Troutdale Municipal Code specifies that abutting property owners are responsible for maintaining the sidewalk on their street frontages. The applicant will be required to cure any existing defects in the existing sidewalks on its frontages prior to obtaining a Certificate of Occupancy.
24. 257th Avenue is a Multnomah County Road. Any requirements associated with improvements, ROW or easement dedications, or access on 257th shall be the purview of the County.
25. Public Utility Easements are required along property frontages. See proposed condition 9.
26. Per TDC Chapter 5, if the public improvements associated with the development exceed \$25,000 in estimated cost, the applicant must complete the public improvements prior to receiving building permits for the onsite buildings. The applicant may seek a waiver of this restriction as provided in TDC Chapter 7. See proposed condition 10.

Proposed Conditions

1. The applicant shall pursue and complete the process for the partial vacation of SE 2nd Street as depicted on the applicant's preliminary plans prior to receiving building permits for the site development.
2. The applicant shall submit with the building permit application an analysis by an engineer licensed in the State of Oregon assessing the adequacy of capacity of the sewer main in Kendall to its intersection with the main in Historic Columbia River Highway, considering the discharges of the proposed development as well as estimated discharges from other users of that subject main.
3. The applicant shall provide a Stormwater Management Report with the building permit applications that summarizes the stormwater design calculations and infiltration capacity of the soils, addresses the stormwater hierarchy, and addresses Source Control requirements, among other typical and customary elements of the SMR.
4. Applicant shall provide with the building permit application a supplement to the Trip Generation and Distribution Memorandum addressing trip distribution to the intersection of 4th and Buxton, daily and peak hours.
5. Applicant shall construct any/all remaining half-street improvements on SW 4th Street along the property frontage.
6. Applicant shall dedicate an easement to the City from the existing stub of SE 2nd Street to SW 257th Avenue for the future 2nd Street multi-use path, of sufficient width to accommodate the planned path and any necessary slopes.
7. The applicant shall dedicate an access easement to the City, for City operations, through the parking/drive aisle between SW Kendall Avenue and SW 4th Street, prior to receiving a Certificate of Occupancy.
8. The applicant shall construct sidewalk ramps meeting current ADA/PROWAG standards at the intersection of SW 2nd and Kendall with the sidewalk infill along Kendall. Design of the northern



CITY OF TROUTDALE ENGINEERING DIVISION

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ramp must be coordinated with the design of the opposing ramp that will be constructed by another developer.

9. The applicant shall dedicate 8' wide utility easements along all property lines that abut City rights-of-way, excepting that where the setback is less than 8' the easement may be reduced to the width of the setback.
10. If the estimated cost of the public improvements is \$25,000 or more, the applicant shall complete the public improvements and receive a Certificate of Completion from Public Works prior to receiving building permits for the onsite buildings; unless the applicant requests and is granted a waiver as provided in TDC Chapter 7.

TO: Alex Lopez, City of Troutdale Assistant Planner

FROM: Chet Hagen, District Administrator

DATE: July 25, 2022

SUBJECT: LU-0013-2022 Home Forward Comments

Alex:

Per the District's Administrative rules, the applicant will be required to submit a [Lighting Level Review form](#) and photometric calculations to assess the lighting levels for all public streets within their proposed project area.

The current lighting level for all public streets must meet Districts standards outlined on page 9, Section 5.120 of our [Administrative Rules](#). If District standards are not met, the applicant will need to enter into a [Construction Permit](#) with the District to bring the lighting levels to current District standards.

Regards,



Chet Hagen
District Administrator

PROJECT #: PRE0001-2022 Home Forward NE 257th Dr & SW Kendall Ave

FROM: Samantha Chandler, Fire

DATE: 1/28/22

FIRE COMMENTS: 94 Unit, 3 3-Story Apartment Buildings

NOTE: Building permit plans shall include a separate "FIRE ACCESS AND WATER SUPPLY PLAN" indicating all of the following

1. Provide fire flow per Oregon Fire Code Appendix B. Fire flow for multifamily buildings varies based on construction type and square footage. **OFC App B Table B105.1.**
2. Temporary addresses of 6" shall be provided at EACH construction entrance prior to ANY construction materials arriving on site. Prior to building finals, a 6" permanent address placard will be required per the Gresham Fire Addressing Policy. **OFC 505 & 1401**
3. Required fire hydrants and access road shall be installed and approved PRIOR to any combustible construction material arriving on site. **OFC 1412.1**
4. Without knowing the building construction types or sizes, a PUBLIC fire hydrant is required to be within 225 feet of the main entrance driveway. The furthest point on each building shall be no more than 400 feet from a hydrant. Show on the building plans where the nearest existing and new hydrants are located. **OFC Appendix C and 507**
5. Each public or private fire hydrant used for fire flow for this property shall have a 5-inch **Storz** adapter with National Standard Threads installed on the 4 ½ -inch fire hydrant outlet. The adapter shall be constructed of high-strength aluminum alloy, have a Teflon coating on the seat and threads, and use a rubber gasket and two (2) set screws to secure it in place. The adapter shall be provided with an aluminum alloy pressure cap. The cap shall be attached to the hydrant barrel or Storz adapter with a cable to prevent theft of the cap. Adapter shall be Harrington HPHA50-45NHWCAP or equal approved by Gresham Fire.
6. The building is required to be provided with fire sprinklers throughout. **OFC 903**
7. A fire hydrant shall be within 50 feet of the fire sprinkler system "FDC". **OFC Appendix C 102.2 & NFPA 13E**
8. Fire hydrant locations shall be identified by the installation of reflective markers. The markers shall be BLUE. They shall be located adjacent and to the side of the centerline of the access roadway that the fire hydrant is located on. In case that there is no center line, then assume a centerline, and place the marker accordingly. **OFC 508.5.4**
9. All Fire Dept. Access Roads shall be drawn to scale and shown clearly on plans. The access roads shall be constructed and maintained prior to and during construction. The minimum width is 26' wide for locations where buildings are over 30' in height. Access roads in areas where fire

hydrants are located are required to be a minimum width is 26' for a length of 20'. Clear height is a minimum 13' 6". **OFC 1410, 503.2.1 & D103.1**

10. The required aerial fire access roads shall be located a minimum of 15 feet and a maximum of 30 feet from the building and shall be positioned parallel to one entire side of the building. This will be required to be approved by the fire code official. **OFC App D-105.3**
11. Required Fire Dept. Access Roads on site shall be designed to support an apparatus weighing 75,000 lb. gross vehicle weight. Provide an engineer's letter stating the access road meets those requirements at time of building permit submittal. **OFC, Appendix D, Section D102.1**
12. The turning radius for all emergency apparatus roads shall be: 28' inside and 48' outside radius. **OFC 503.2.4**
13. No Parking Fire Lane signage or curb marking may be required. Fire access roads 20' – 26' wide require the marking on both sides. Indicate on the building permit plans. **OFC D 103.6**
14. If a gate is installed on a fire access road, it must meet the requirements of the Gresham Fire Gate Policy. **OFC 506.1**
15. A fire alarm system will be required. **OFC 907, 903.4**
16. Prior to applying for a building permit provide a fire flow test and report. The fire flow report will verify that the correct fire flow is available and will be required to have been conducted within the last 12 months. **OFC 507.3 & B-101.1**
17. A KNOX box will be required at the fire sprinkler riser room. If temporary construction gates are used, a KNOX exterior padlock will need to be installed for the duration of its use. These can be purchased at knoxbox.com/greshamfd **OFC 506.1**
18. Class III standpipe required throughout buildings where the floor level of the highest story is located more than 30 ft above the lowest level of the fire department vehicle access. Shall be installed per NFPA 14. **OFC 905.3.1**
19. The fire apparatus access shall extend to within 150 feet of all portions of the exterior walls of the first story of the building as measured by an approved route. **OFC 503.1.1**
20. Fire apparatus access roadway grades shall not exceed 10%. Intersections and turnarounds shall be level (maximum 5%). Where fire apparatus access roadway grades exceed 10% but no more than 12%, buildings are required to have a fire sprinkler system installed. **OFC 503.2.7, D103**

1620 SE 190th Avenue, Portland, Oregon 97233-5910 • Phone (503) 988-5050 • Fax (503) 988-3321

MEMORANDUM

TO: Home Forward, applicant
Alex Lopez, City of Troutdale

FROM: Jessica Berry, Transportation Planning and Development Manager

CC: Graham Martin, Transportation Planner (contact: row.permits@multco.us)

DATE: August 8, 2022

ADDRESS: SW 257th Dr & SW Kendall Ave (R649791090; R649790500; R649790490)

SUBJECT: **LU-0013-2022 Home Forward Troutdale – County findings and conditions.**

Multnomah County Transportation Planning and Development has reviewed the above referenced transportation review request and provides the following comments.

The comments provided in this memorandum are based on the information submitted to the City of Troutdale. While every effort has been made to identify all related standards and issues, additional issues may arise and other standards not listed may become applicable as more information becomes available.

The applicant seeks to build a 94-unit affordable housing project. Vehicular access to the site will be from Kendall Ct. and SW 4th St, both City of Troutdale local streets. The proposal includes frontage on SW 257th Ave, a County maintained Major Arterial facility.

On the following pages, all references to *Multnomah County Design and Construction Manual* (MCDCM) use the acronym “MCDCM” and all references to *Multnomah County Road Rules* (MCRR) use the acronym “MCRR”. Numbers correspond to the relevant sections within the MCRR or MCDCM.

CONDITIONS OF APPROVAL

The following conditions of approval will need to be met as soon as possible. At a minimum, the conditions will need to be met prior to the submittal of the Building Permit to Troutdale.

1. MCRR 7.000: The applicant must provide a Transportation Impact Study or Assessment that addresses the impact of the development on the level of service and capacity of the following intersections. This needs to be submitted to Multnomah County prior to Building Permit application:
 - i. Cherry Park Rd /Buxton Rd;
 - ii. East Historic Columbia River Highway/Buxton Rd

- iii. Buxton/2nd
 - iv. Buxton/4th
2. MCRR 8.000 The applicant may be required to provide off site improvements to mitigate impacts at intersections based on the information provided in the Transportation Impact Study.
 3. The applicant is required to contact Mid-County Lighting District (chet.hagen@multco.us) to determine if street lights are needed. If street lights are required by the lighting district; this will need to be shown on plans at the time of Building permit application.
 4. MCRR 26.000 Applicant must provide a [stormwater certificate](#), and accompanying report with calculations, to Multnomah County (row.permits@multco.us) for review. Review required to ensure that stormwater does not impact County Transportation facilities. Fee for stormwater review \$200.

FINDINGS

ACCESS STANDARDS

As access is not proposed onto 257th, County access standards MCRR 4.300-4.500 do not apply.

MCRR 5.000 Transportation Impact

MCRR 5.100 To determine if a Transportation Impact is caused by a proposed development, the County Engineer will determine the number of new trips generated by a site by one of the following methods:

- A. Calculations from the most recent edition of the Institute of Transportation Engineers' Trip Generation (ITE); or*
- B. A site development transportation impact study conducted by a professional engineer registered in the State of Oregon and accepted by the County.*

The Multnomah County Road Rules defines a Transportation Impact as the effect of any new construction or alteration which will increase the number of trips generated by a site by more than 20 percent, by more than 100 trips per day or by more than 10 trips in the peak hour [MCRR 3.000]. A minimum increase of 10 new trips per day is required to find a transportation impact.

Staff Comments

The Trip Generation and Distribution Memo notes that the site will generate 452 weekday daily total trips. Per the definition of a transportation impact, the proposal will generate a transportation impact. However, further information is required to determine whether any off-site improvements are required. See MCRR 7.000 and MCRR 8.000 below.

MCRR 7.000 Transportation Impact Study

The County Engineer may require that a transportation impact study be submitted to the County as a part of a land development proposal at the Engineer's discretion. The scope of the study will be set by the County Engineer and by the standards in the Design and Construction Manual. The County may develop conditions of approval based upon the findings of a traffic study, but the County is not bound by those findings.

Staff comments

Multnomah County Memo EP-2022-15487 noted that the developer needed to submit Transportation Impact/Trip Generation Memo to take account of the current and build out conditions at several intersections. In addition to the information presented in the Trip Generation and Distribution Evaluation Memorandum prepared by Dana Beckwith (Oregon PE 53044), the County also would like the applicant to evaluate the Level of Service (LOS)/capacity impacts on the intersections include in the Memo as well as the following intersections:

- Buxton and E Historic Columbia River Highway
- Buxton and Cherry Park Road
- Buxton and 4th
- Buxton and 2nd

Off-site Improvements (MCRR 8.000)

It is County policy to require off-site improvements as a condition of a site development permit to satisfy safety requirements, development created capacity needs, County road maintenance requirements, Uniform Fire Code requirements, ADA requirements and other public service requirements, and to protect the public from the detrimental effects of a proposed development.

Staff Comments

The analyses included in the TIS (noted above: MCRR 7.000) will be used to determine whether the County requires any off-site improvements to mitigate the impact of the new trips on the County transportation network. It is important to include all intersections in order to determine if the new development creates congestion issues that may impact the safety of all users.

Mid-County Lighting District

Contact Mid-County Lighting District to determine if street lights are needed. The property is located within the Mid-County Street Lighting District and may require lights along the frontages. The applicant may need to develop street lighting design that is compliant with Mid-County Lighting District standards. Please contact Chet Hagen, Mid-County Street Lighting District Administrator, at chet.hagen@multco.us or 503-988-0164 for more information.

Staff Comments

It is the responsibility of the applicant to communicate with the Mid-County Lighting District. It is not clear if this has been done yet.

Stormwater Management

Multnomah County Transportation requires any stormwater feeding into the public ROW to be built to a 25-year storm event (Multnomah County Road Rules, 26.300, Stormwater Discharge permit requirements; Multnomah County Design and Construction Manual, 5.1.2 Water Quantity Design Standards).

Staff Comments

Applicant will still need to provide Stormwater requirements. The fee for stormwater review is \$200. The certificate form can be found at the following webpage: <https://multco.us/file/64172/download>

Alex Lopez

From: Wm Wood <GimbalLock@msn.com>
Sent: Monday, August 8, 2022 5:09 PM
To: Community Development
Cc: Denise Wood
Subject: comment regarding Windust property

My response to this plan is quick, and I hope, legible.

Let's see....First I had it explained to me that the Windust Property would be the site of a new City government building, allowing the addition of a dedicated drive between the Public Works building site and the new City Hall that would provide City vehicles better access and alleviate the strain on 4th Street. Sure thing. Then I hear that the construction of an apartment complex on the site would only need an access drive for fire vehicles. And now, predictably, that access drive is now going to be a dedicated access drive for residents of the apartments. A drain onto a wholly inadequate street for the proposed use. 4th Street has no sidewalks, is narrow and is a danger to anyone walking on it. I can hear it now. "Let's restrict parking to one side of the street. They won't mind...."

An apartment complex should NOT be granted any kind of variance from parking or any other ordinance, because despite today's best intentions or delusions, things change. Currently it is envisioned that the apartments have a specific purpose and use. Two or three elections away that can magically be deemed infeasible or lobbied into history. Yet the variance will still be in effect and the burden on the neighboring residents will be real and undeniable.

Next thing you know someone will want to build a new community police building for our City. Then, before the paint is even cured there'll be talk of letting the County take it. That isn't what we voted for. Some of us are thinking of the song "We won't get fooled again."

New commercial projects are never adequately held accountable for the impact to the infrastructure or the quality of life in surrounding residential areas. That will be the case for this ill-advised project. Has anyone analyzed the adequacy of water pressure or volume when that complex is complete and occupied? Traffic flows have been ignored, obviously. Or care for the kids that will be negotiating the jungle on their way to school.

The addition low-income housing is a necessary and valuable component of a balanced society. But the location of the building on a wedge of land plagued by noise and air pollution, and without adequate access and protection for its residents, is a slap in the face to those that envision a genuine improvement to the plight of many struggling to make it.

Wake up, folks. You're being hoodwinked by the County. Again. DO NOT GRANT ANY VARIANCES FOR THIS PROJECT. IT SHOULD'NT EVEN HAVE BEEN ENTERTAINED AT THE START.

Sincerely,
Bill Wood
107 SW 4th Street
Troutdale
GimbalLock@msn.com

James M. Gurtisen, OD
133 SW 4th St.
Troutdale, OR 97060
{Phone/Fax: 503-492-2338}

FAX

TO: Planning Dept FROM: Jim GURTISEN
FAX: 503 616 7-0524 FAX: 503 492 2338
PHONE: _____ PHONE: _____
SUBJECT: Comment re: DATE: 8 Aug 22
LU-0013-2022

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PAGES

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Item 1: I submit that the Home Forward Project should never have been even considered from day one.

Frank Windust was the original owner of the two lots being developed. He was well known in Troutdale as a developer and realtor. I believe he is deceased at this time. He held the two lots for many years as he was looking for the proper buyer that would use the property in the best interest of Troutdale. When he sold the property to Multnomah County, he expressly asked that it not be used for subsidized housing. He felt so strong about this that he had to wait for many years to find just the right buyer. Multnomah County wanted the lots for a East County Policing Facility/ offices and Mr. Windust saw this as a great opportunity to fulfill his dream for the lots. How does Multnomah County justify breaking the cornerstone of his agreement by substituting his worst night mare (subsidized housing) for a decoy project (Sheriff's offices). I am not that cynical that I believe this was a devious plan to obtain his property. I realize that plans change, The Sheriff instead decided to use the Troutdale police station. To honor the deal Multnomah County had with Mr. Windust they should have torn down the old, vacant East County Sheriff's offices on 122nd ave. and built Home Forward there.

I propose that Home Forward Troutdale be abandoned and be replaced with a new Home Forward project and the old East County Sheriff admin. Offices on 122nd. This would be a much better fit.

Item 2: There are too many units planned for the Troutdale Home Forward site. Most of the problems everyone is having with this project is trying to do too much with too little land and jamming it into a site that is not appropriate for human habitation in the planned density. There are so many units planned for this project that everyone living, adjoining to, or connected with this area will realize a negative impact. I have not met one person who has a positive impression of this project.

Item 3: When the Troutdale Police Headquarters was built promises were made by Captain Anderson that were not kept. One of them was to honor the value of the trees that were on the property. Every single one of the 250 trees that were on the property were slashed down, without proper review by the two committees that are supposed to represent the "City of Trees". City Ordinance "13.10.270" addresses concerns about tree removal. I suggest that "the Director" honor the existence of the valuable trees that are on the home Forward building site. How do the citizens know that the "the Director" even values any trees, or the animals that use these trees? Will there be any concern about how our citizens feel about losing another stand of trees that will probably go down on this project. The Metro area is removing one percent of it's tree coverage every year. Metro is asking us private citizens to save our trees but are they going to honor their obligation to do likewise. Troutdale is in a position to take a stand on this issue as we have rules here that could really justify Troutdale as a "City of Trees".

Item 4: The Multnomah Sheriffs do not have the ability, energy, or directive to police the noise and speeding on 257th and Buxton. The Policing, regarding speeding and vehicle noise and nonviolent crime, is nonexistent as far as this area is concerned. I live on 4th and Buxton and have watched the Police give up. Our city code enforcer is not able to keep up either, and we are going to jam another 2 to 3 hundred more people into the planned 3 buildings. Do you really think these new people are going to care about our city? Our neighborhood is fraught with anxiety because the current infrastructure will not handle the load.

JIM GURTISEN

Item 5: Noise, overhead commercial airplanes, helicopter pilots training overhead, I-84 noise, Railroad noise, Trucks abusing jake brakes on 257th, speeders going up 257th in un-muffled cars and trucks and motorcycles, right next to the Home forward project. People around this neighborhood don't have to stay inside to avoid the din and people in the Home Forward Project homes will see the worst of it.

Item 6: Air Pollution, The Home Forward project sits right in the middle of a transportation snarl that dispenses a constant dose of diesel particulates and exhaust gasses. There are about 200 deisel trucks that inhabit the area next to the freeway. Truckers don't like to turn off their trucks, Offshore wind blows this exhaust right over the Home Forward neighborhood. Anyone with lung disease should stay away from this project.

JIM GURTISEN

Alex Lopez

From: Denise Wood <Nerfangel1@outlook.com>
Sent: Monday, August 8, 2022 4:34 PM
To: Community Development
Subject: LU-0013-2022 Home Forward Type III Variance

August 8, 2022

To: City of Troutdale

Regarding File Number/Name: LU-0013-2022 Home Forward

Type III Variance, Site Development Review, Lot Line Adjustment.

In regards to the Home Forward development there is much to be concerned about.

1. Location: The location chosen for the Home Forward apartment complex is inappropriate as living space. It would better serve the Troutdale community and downtown businesses to have a parking lot on this site. A parking lot would allow for more access to the down town businesses year round, especially during the holiday seasons. As well as parking for the future development along the river walk that the city has planned. I don't need to tell the city that they are in need of a parking lot that can hold a hundred or more cars. The latest First Friday event, Antique Car show, and other events that clog Buxton and all the streets surrounding the down town corridor are an excellent example of why we need more parking access to our main street businesses. If nothing else, the proposed building site would be better fit for more businesses or for its original purchase as a site for Multnomah County office building.
2. Reasons that the site is inappropriate for housing:
 1. There is no access to grocery stores that offer a large variety of fresh produce or fruit within a mile of this site that can be accessed by foot.
 2. There is no developed community Park for residents or children to access within the neighborhood of the proposed apartment site.
 3. There is no developed outdoor area for teen's to access for activities.
 4. Almost continuous traffic that pollutes and create unacceptable noise levels.
 5. No lighted cross walks off of 2nd or 4th street for children to cross Buxton when walking to school.
 6. Apartment parking variance request that would be an impact to current residents and business.
 7. There are no continuous sidewalks along SW 4th Street.

Specific concerns:

1. Traffic: As any resident that lives in proximity to Buxton and SW 4th Street or SW 2nd street can attest to, getting onto Buxton has been an issue. Especially at commute times. During commute hours (7:00am-8:30am) and (3:00pm-6:00pm) you currently have to go south on Buxton, then go west onto Cherry Park just to go North onto 257th. I'm concerned that without a 4 way stop or lighted cross walk on SW 4th Street and Buxton, access to Buxton with a "reasonable" delay will become even more

difficult, thus impacting both Residents and Troutdale City Water Works and other city vehicles ability to access north/south bound Buxton.

2. Request for parking variance and traffic flow onto and off of SW 4th/SW 2nd Street by Home Forward.

My concern is that SW 4th Street has heavy traffic and parking already. With current city vehicles, delivery trucks and resident vehicles traversing it; proposing to add in a mix of children, foot traffic on a street that doesn't even have sidewalks along its entire length, and 100 + cars accessing it is not a good mix. None of the current residents along SW 4th Street are in agreement with this proposal. I am concerned about that and especially the cities need to access Buxton and other roads during our winter storm events. Home Forwards proposal is for 94 one to three bedroom units, at 2 cars minimum per unit, that requires 188 parking spaces, if they add visitor parking at only 6 spaces that would require parking for 203 cars. I understand that the current design for parking within the apartment complex is somewhere near 127 spaces. And at this time they are asking for a variance from the city that exceeds what is currently allowed for offsite parking. Which leaves Buxton, residents on SW 4th, SW 2nd Street as well as businesses in the area of SW 2nd and Kendall Court impacted by the over flow parking necessary to access the apartment complex.

3. I am especially concerned about foot traffic for children from the proposed apartment complex

This could mean we would have children trying to cross Buxton Street at SW 4th Street during commute times, as they try to get to Troutdale Elementary School. SW 4th street doesn't even have continuous sidewalks along it for pedestrians.

4. The site design doesn't even include fencing of the apartments along 257th; I am concerned for the residents who might live there and their children.
5. I don't see plans for a retaining wall between the resident at the end of SW 4th street and this building site. Trees and plants will not retain the soil in that area and overtime erosion could undermine the yard or the home of the current resident. Will Home Forward put in a retaining wall to protect the home owner's property value?

Comment: I know that my concerns are shared by many, please understand that I am in favor of affordable Housing, it is necessary and needed. But when I stand on Kendal court and 2nd street, or SW 4th and Buxton at morning or evening commute times, 257th and Buxton are both heavy with traffic, the noise level and the exhaust fumes from trucks and other vehicles is over powering. Traffic is already a concern to those of us who live in this area, adding over 100 people seems unwise and unworkable. Look how long it took just to get a 3 way stop at the bottom of Buxton and Columbia Scenic Hwy.

Is this the best Multnomah County can do as a site that will house for over a hundred people. I think the County can do and must do better for families than this. Ask yourself if you would raise a family there.

Sincerely,

Denise J. Wood

Troutdale Resident

Alex Lopez

From: Paul Peterson <pconradp@hotmail.com>
Sent: Monday, August 8, 2022 2:15 PM
To: Community Development
Subject: NO NO NO to "94 affordable apartment homes"

Good day,

My wife and I are residents of downtown Troutdale and we DO NOT want to see the precious greenspace at SW Kendall Ave and SW Second Street developed into affordable housing.

- 1) there is already too few parking spots downtown and that greenspace was used this weekend for overflow parking during the Saturday event so it does not make any sense to develop it with more cars
- 2) the town does not need affordable housing
- 3) the small town feel needs to remain and 94 affordable apartments does not keep to the small town feel
- 4) terrible location for housing as traffic congestion already exists in the area
- 5) housing values will decrease with "affordable housing"

We are opposed to any development on that lot and believe it should remain as greenspace.

The people in this town moved here for the small town feel and do not want the development to continue.

Paul & Teresa

To: Troutdale City Council, Mayor, Planning Department & City Manager Ray Young

I am Mrs. Zelma Sutherland. I live on 225 SW 4th St. Troutdale. I have lived here at the same house for 56 years. I object to the 94-unit housing development in three story buildings by Home Forward. It will devalue my property as well as block my view I have had for 56 years.

Home Forward will have to build two retaining walls around the north and west sides of my property with a chain link fence on top. There is not enough parking for that many units. We are afraid they will end up parking on 4th Street in front of our homes, which is the main road to the city Water Dept.

Has the air quality been checked for our neighborhood? We {are} 4th St, Buxton Road- 257th-Main St. Troutdale Food Cart parking lot.

Does the city have enough water and services and sewer for this large of unit?

We went through this same problem when Mr. Windust wanted to build the same kind of housing. He took all the topsoil off and sold it. The weather is a very big concern. The East wind blows all year long. Hot-cold-wet-snow-ice! {The wind} can blow roofing from units onto 257th traffic! As well as can freeze your front door shut.

Wrong place for this type of project!!!

To: Troutdale City Council; Mayor & Planning Dept.

I am Mrs. Zelma Lutherland I live on 225 S.W. 4th St. Troutdale. I have lived here for 56 years same house.

I object to the 94 unit housing development in three story buildings by home forward.

It will devalue my property as well as block my view I have had for 56 years.

Home forward will have to build two retaining walls around the north and west sides of my property with a chain length fence on top.

There is not enough parking for that many units. We are afraid they will end up parking on 4th Street in front of our homes which is the main road to the City water Dept.

Has the air quality been checked for our neighborhood? We 4th St. Buxton road - 257th - main St. Troutdale - food cart parking lot.

Does the city have enough water and services and sewer for this large of units?

We went through this same problem when Mr. Mindust wanted to build the same kind of housing. He took all the top soil off and sold it.

The weather is a very big concern. The East Wind blows all year long. Hot - Cold - Wet - snow ice!

Which can blow roofing from units on to 257th traffic, as well as freeze your front door shut.

Wrong place for this type of project!!!



Findings of Fact & Final Order

HEARING & DECISION

August 24, 2022

DATE:

FILE NUMBER / NAME	LU-0013-2022 Home Forward		
APPLICATION TYPE	Type III Variance, Site Development Review, Lot Line Adjustment		
PROJECT APPLICANT	Taryn Wheeler, MWA Architects	PROPERTY OWNER	Multnomah County
PROJECT LOCATION	SW Kendall Ct & SW 2 nd St	TAX MAP / TAX LOT #	1N3E25CB -02501 / R649790500 1N3E25CB -02601 / R649791090
LAND USE MAP	Commercial (C)	LAND USE ACTIVITY	Vacant
ZONING DISTRICT	Downtown Mixed-Use (MU-1)	OVERLAY DISTRICT	Town Center, Slope District

PROPOSAL

The Applicant applied for a Type III Variance, Site Development Review, and a Lot Line Adjustment on the subject property for a 94-unit affordable multi-family housing development in three, three-story buildings. The application was elevated to a Type III with the request of a variance to the off-street parking standards, the site development review is to ensure the design standards for multi-family housing is met and the lot line adjustment will remove the existing property line between the two tax lots.

PROCEDURE

This application underwent a Type III quasi-judicial procedure. [TDC 2.060] This procedure requires a Public Hearing and Planning Commission review in order to be adopted. Planning Commission was the decision-making body for this application and may approve, approve with conditions, or deny this application. Nearby property owners, relevant review entities, and other stakeholders were notified accordingly. [TDC 2.075 - 2.090]

APPLICABLE CRITERIA

Listed below are governing standards that applied for this application:

- *Troutdale Development Code (TDC):* Troutdale Development Code (TDC): **Ch. 1** Introductory Provisions; **Ch. 2** Procedures for Decision Making; **Sec. 3.211** Downtown Mixed-Use (MU-1); **Sec 4.300** Vegetation Corridor and Slope District (VECO); **Sec 4.600** Town Center; **Sec. 5.600** Erosion Control and Water Quality Standards; **Sec 5.700** Stormwater Management; **Sec. 5.1000** Public Improvements; **Sec 6.1300** Variance; **Ch. 7** Lot Line Adjustments; **Ch. 8** Development and Design Standards; **Ch. 9** Off-Street Parking and Loading; **Ch. 17** General Provisions; **Appendix A:** Design Standards for Central Business District
- Troutdale Municipal Code: 8.26 Outdoor Lighting
- City of Troutdale Comprehensive Land Use Plan
- Town Center Plan 2020-2040
- Construction Standards for Public Works Facilities
- Building and Fire Codes

FINDINGS OF FACT

- Planning Commission received public testimony from Staff, the Applicant, and members of the public at the August 24, 2022 hearing.
- The Findings of Fact contained herein are derived from the specific decision criteria outlined in Sections 6.900, 6.1300 and 7.180 of the Troutdale Development Code and are hereby adopted as the Findings of Fact in this matter.
- The Final Order has been reached and _____ of the application **with conditions** is hereby issued.

FINDINGS OF FACT – VARIANCE TO REQUIRED PARKING [TDC 6.1315]

6.1325 The Planning Commission may grant a variance under the Type III procedure if the request involves the expansion or reduction of a quantifiable provision in this Code by more than thirty percent (30%), or if the request is referred to the Planning Commission in accordance with Section 6.1335 of this Chapter. The variance shall be granted only if the Planning Commission determines that the criteria in Section 6.1315 of this Chapter are met:

- A. Special circumstances or conditions including, but not limited to, lot size, lot shape, topography, or size or shape of building, apply to the property, development, or to the intended use and are not typical of the general conditions in the surrounding area; and**

FINDING: The site features some significant slopes towards the southern property lines that are not typical or do not affect surrounding properties as significantly due to land use. The topography of the site creates spatial challenges, specifically to developing the layout of the buildings on site. In turn, this influences the amount of parking able to be provided and the ability to fit said parking on the site given other requirements such as landscaping. **As such, this criterion is met.**

- B. The variance authorized will not be injurious to adjacent properties or the surrounding neighborhood or otherwise detrimental to the public welfare; and**

FINDING: The applicant has submitted a parking study that was reviewed by Multnomah County Transportation and the City of Troutdale Public Works Department. Both agencies have proposed Conditions of Approval for right-of-way improvements, dedications and/or easements as necessary. The applicant has also provided a Trip Generation and Distribution Evaluation for this development. Multnomah County Transportation has requested as a Condition of Approval that a Transportation Impact Study or Assessment be submitted prior to Building Permit application.

The applicant has proposed 119 total off-street parking spaces. With 94 units, the required number of spaces would be 219. The applicant has proposed 11 on-street parallel parking spaces per 9.040.C, which reduces the required number of parking spaces to 208. The amount of parking works out to a ratio of 1.27 spaces per each unit (42.7% variance). The alternative with the extra parking area on site, brings the total parking provided to 144 spaces for a ration of 1.53 spaces per each unit (30.7% variance). The application states that the owner and developer has found, through parking demand studies of comparably sized affordable housing sites in Troutdale and Fairview that there is an average overnight parking demand of 1.09 spaces per unit can be expected to be used. The property is also located along two transit lines with service seven days a week.

In addition to the Applicant’s testimony, City Staff has researched comparator and surrounding apartment complexes and found that the typical utilization rate of parking is comfortably below the amount of parking provided, ranging from 0.83 to 1.49 occupied spaces per unit, suggesting that a 1.27 or 1.53 spaces per unit will adequately serve the needs of future residents and visitors and would not have adverse negative impacts on adjacent properties. **As such, this criterion is met.**

- C. The variance authorized will be consistent with the general purpose and intent of the provision from which a variance is sought; and**

FINDING: The intent of the provision is to provide adequate parking for multi-family affordable developments and to limit potential spillover parking in adjacent neighborhoods.

Using the Institute of Transportation Engineers *Parking Generation Manual 5th Edition* which provided parking generation rates for affordable housing development, shows for overnight parking (which is the highest parking demand period for a residential development), for 94 units, 1.33 spaces per unit occupied is in the 85th percentile, which is in between the two potential parking ratios for this development.

Given the data and relevant experience, the variance complies with the intent of the provision to provide sufficient parking for the development. **As such, this criterion is met.**

D. The variance is the minimum necessary to relieve a practical difficulty with full compliance and to avoid or minimize the resulting hardship.

FINDING: The applicant appears to have made every effort to include as much parking as possible by including a potential layout option that will add additional spaces while also meeting density, landscaping, and other requirements for the site. **As such, this criterion is met.**

FINDINGS OF FACT – SITE DEVELOPMENT REVIEW [TDC 6.915.B]

6.915 Approval Criteria. In order to approve a site development review application, the decision making authority shall make findings of fact based on evidence provided by the applicant demonstrating that the proposal is consistent with the applicable approval criteria.

Section 6.915.B

An application for a Type II site development review shall be approved if the proposal meets all of the following criteria. The City decision-making body may, in approving the application, impose reasonable conditions of approval, consistent with the applicable criteria.

1. The proposal complies with the approval criteria specified in Section 6.920.A.

- 1. The application complies with all of the applicable provisions of the underlying zone and overlay zone(s), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards, or a variance or adjustment is granted.**

FINDING: The application meets lot area, dimensions, density, building orientation, and lot coverage standards. There is no applicable floor area requirement. Architectural standards largely appear to be met through outright approval or through an administrative adjustment or during building plan review. All setback requirements have been met for this development. A provision within Senate Bill 8 allows the affordable-housing development to go over the maximum height limit of 35'. **As such, this criterion is met.**

- 2. The proposal includes required upgrades, if any, to existing development that does not comply with the applicable land use district standards, pursuant to Section 5.300 Nonconforming Uses.**

FINDING: This proposal does not contain any required upgrades to any existing development that is non-conforming since the property is vacant and has never been developed. **As such, this criterion is met.**

- 3. The proposal complies with all applicable site design and development standards of this Code, such as landscaping and parking.**

FINDING: At 94 units, 219 parking spaces would be required. However, the applicant has proposed 11 adjacent on-street parking spaces per TDC 9.040.C, the required number of off-street parking spaces can be reduced by one (1) parking space per every on-street parallel parking space located adjacent to the subject site. There are five (5) on-street spaces proposed along SW Kendall Ct and six (6) spaces proposed along SW 4th St. Therefore, a total of 208 off-street parking spaces are required for the development.

The applicant has proposed a total of 119 off-street parking spaces (1.27 spaces per DU) for the site, which is a 42.7% variance to the off-street parking requirement. The applicant has also included a secondary option (shown as Option C in the plans provided), to include 25 additional spaces on the northern panhandle area of the property. This option would provide a total of 144 off-street parking spaces (1.53 spaces per DU), which is a 30.7% variance to the off-street parking requirement.

Based on the submitted plans, it appears that all other parking standards can be met. The applicant shall demonstrate in their building plans that the parking landscaping standards within TDC 9.045 are met, and that the amount and dimensional requirements for bicycle parking are met per TDC 9.080. See Planning Division Condition of Approval #4.

Per TDC 8.060.A.1, the minimum area of a site within the MU-1 zoning district to be retained in landscaping is 5%. The applicant has proposed for 40.4% of the site to be retained in landscaping, far above the minimum required. All required open areas around property lines and the roadway are shown on the landscaping plan to be landscaped. Additionally, the southern corner of the property that abuts the

residential area along SW 4th St will have additional area for sight-obscuring landscaping to help provide a buffer between the development and the adjacent neighborhood. All landscaping requirements that are applicable per TDC 8.060 shall be met by the time of building permit submittal.

All standards for pedestrian access and circulation have been met by this application per TDC 8.150.B.3.a-d. Onsite pedestrian circulation shall be continuous and connect to streets abutting the site. The garbage and recycling enclosures will be screened with 6' tall concrete blocks and privacy slats with mechanical gates.

The applicant has provided plans that show where trees are to be removed for the development that are in areas primarily where the parking lots and internal circulation roadway will be along the perimeter of the subject property. The buildings themselves are proposed to be in areas where no trees exist currently.

As such, this criterion is conditionally met.

4. **The proposal meets all existing conditions of approval for the site or use, as required by prior land use decision(s), as applicable.**

FINDING: There are no prior land use decisions for the site. **As such, the criterion is met.**

2. **If applicable design standards are proposed to be adjusted, the proposed adjustment:**

- a. **Is justified due to unique site conditions.**
- b. **Conforms to the extent practicable with these design standards.**
- c. **Mitigates potential impacts from the adjustment to the extent practical.**

FINDING: The applicant has proposed six (6) administrative adjustments to design standards. The adjustments are to the standards found in 8.150.B.4a, 8.150.B.4.f, 8.150.B.f.d.ii, 8.150.B.6.a, 8.150.B.6.b and A.109.C. The standards have been found to be reasonable and justified due to the site conditions and/or conforms to the extent practicable with these design standards. All of the adjustments will help retain a cohesive design language and provide added privacy for residents of the development and in the adjacent homes. **As such, this criterion is met.**

FINDINGS OF FACT – LOT LINE ADJUSTMENT [TDC 7.180.D]

7.180.D Approval Criteria. The applicant must demonstrate that the proposed lots conform to the applicable zone as provided above, and that no boundary line will violate the applicable setbacks; that each proposed reconfigured lot complies with the access and sight distance standards of this Chapter and the City street standards.

FINDING: The proposed lot meets dimensional standards for the applicable zoning district. No boundary line will violate applicable setbacks. The consolidation of the lots will not impact access or sight distance standards. The submitted record of survey does not account for the proposed partial vacation of SW 2nd St. The applicant shall resubmit a record of survey including this item after the right-of-way vacation has been approved. See Planning Division Condition of Approval #3. **As such, this criterion is conditionally met.**

FINAL ORDER & DECISION

FINAL ORDER

Based upon the foregoing Findings of Fact, the Troutdale Planning Commission the Variance, Site Development Review, and Lot Line Adjustment for Case File LU-0013-2022 (Home Forward) on the count of all decision criteria being met, ***subject to the conditions of approval*** as stated herein.

DECISION

The Planning Commission hereby adopts these Findings of Fact as stated and has voted to _____ the Application as described in the Final Order above and subject to the conditions of approval as stated herein.

This decision shall expire two (2) years from the date of this Final Order.

YEAS:

NAYS:

ABSTAINED: 0

ABSENT: 0

Tanney Staffenson, Planning Commission Chair

Date

CONDITIONS OF APPROVAL

City of Troutdale Planning Division

1. Applicant shall be required to satisfy comments made by review entities, included in the attached Findings of Fact, or shall work with Staff to reach an agreement between the parties.
2. The approval of case file LU-0013-2022 shall expire automatically two (2) years from the effective date of the decision unless the approval is enacted either through construction, establishment of use, or recordation of plat or survey within the specified time period. The effective date of the decision shall be the date of the Notice of Decision, unless such decision is appealed.
3. The applicant shall resubmit the lot line adjustment record of survey prior to recordation with the County, which shall include all existing and proposed lot sizes, easements and to reflect the vacation of SW 2nd St.
4. The applicant shall demonstrate in their building plans that the parking landscaping standards within TDC 9.045 are met, and that the amount and dimensional requirements for bicycle parking are met per TDC 9.080.
5. In their building permit submittal, the Applicant shall demonstrate that the applicable architectural requirements of TDC 8.150.

City of Troutdale Building Division

1. Following Land Use approval, the applicant shall submit for a building permit as well as any applicable trade permits through the Building Division CSS portal (<https://troutdaleor-energovpub.tylerhost.net/apps/selfservice#/home>) as per the above provided comments.

City of Troutdale Public Works

1. The applicant shall pursue and complete the process for the partial vacation of SE 2nd Street as depicted on the applicant's preliminary plans prior to receiving building permits for the site development.
2. The applicant shall submit with the building permit application an analysis by an engineer licensed in the State of Oregon assessing the adequacy of capacity of the sewer main in Kendall to its intersection with the main in Historic Columbia River Highway, considering the discharges of the proposed development as well as estimated discharges from other users of that subject main.
3. The applicant shall provide a Stormwater Management Report with the building permit applications that summarizes the stormwater design calculations and infiltration capacity of the soils, addresses the stormwater hierarchy, and addresses Source Control requirements, among other typical and customary elements of the SMR.
4. Applicant shall provide with the building permit application a supplement to the Trip Generation and Distribution Memorandum addressing trip distribution to the intersection of 4th and Buxton, daily and peak hours.
5. Applicant shall construct any/all remaining half-street improvements on SW 4th Street along the property frontage.
6. Applicant shall dedicate an easement to the City from the existing stub of SE 2nd Street to SW 257th Avenue for the future 2nd Street multi-use path, of sufficient width to accommodate the planned path and any necessary slopes.
7. The applicant shall dedicate an access easement to the City, for City operations, through the parking/drive aisle between SW Kendall Avenue and SW 4th Street, prior to receiving a Certificate of Occupancy.
8. The applicant shall construct sidewalk ramps meeting current ADA/PROWAG standards at the intersection of SW 2nd and Kendall with the sidewalk infill along Kendall. Design of the northern ramp must be coordinated with the design of the opposing ramp that will be constructed by another developer.
9. The applicant shall dedicate 8' wide utility easements along all property lines that abut City rights-of-way, excepting that where the setback is less than 8' the easement may be reduced to the width of the setback.

10. If the estimated cost of the public improvements is \$25,000 or more, the applicant shall complete the public improvements and receive a Certificate of Completion from Public Works prior to receiving building permits for the onsite buildings; unless the applicant requests and is granted a waiver as provided in TDC Chapter 7.

Gresham Fire & Emergency Services

1. Provide fire flow per Oregon Fire Code Appendix B. Fire flow for multifamily buildings varies based on construction type and square footage. OFC App B Table B105.1.
2. Temporary addresses of 6" shall be provided at EACH construction entrance prior to ANY construction materials arriving on site. Prior to building finals, a 6" permanent address placard will be required per the Gresham Fire Addressing Policy. OFC 505 & 1401
3. Required fire hydrants and access road shall be installed and approved PRIOR to any combustible construction material arriving on site. OFC 1412.1
4. Without knowing the building construction types or sizes, a PUBLIC fire hydrant is required to be within 225 feet of the main entrance driveway. The furthest point on each building shall be no more than 400 feet from a hydrant. Show on the building plans where the nearest existing and new hydrants are located. OFC Appendix C and 507
5. Each public or private fire hydrant used for fire flow for this property shall have a 5-inch Storz adapter with National Standard Threads installed on the 4 ½ -inch fire hydrant outlet. The adapter shall be constructed of high-strength aluminum alloy, have a Teflon coating on the seat and threads, and use a rubber gasket and two (2) set screws to secure it in place. The adapter shall be provided with an aluminum alloy pressure cap. The cap shall be attached to the hydrant barrel or Storz adapter with a cable to prevent theft of the cap. Adapter shall be Harrington HPHA50-45NHWCAP or equal approved by Gresham Fire.
6. The building is required to be provided with fire sprinklers throughout. OFC 903
7. A fire hydrant shall be within 50 feet of the fire sprinkler system "FDC". OFC Appendix C 102.2 & NFPA 13E
8. Fire hydrant locations shall be identified by the installation of reflective markers. The markers shall be BLUE. They shall be located adjacent and to the side of the centerline of the access roadway that the fire hydrant is located on. In case that there is no center line, then assume a centerline, and place the marker accordingly. OFC 508.5.4
9. All Fire Dept. Access Roads shall be drawn to scale and shown clearly on plans. The access roads shall be constructed and maintained prior to and during construction. The minimum width is 26' wide for locations where buildings are over 30' in height. Access roads in areas where fire hydrants are located are required to be a minimum width is 26' for a length of 20'. Clear height is a minimum 13' 6". OFC 1410, 503.2.1 & D103.1
10. The required aerial fire access roads shall be located a minimum of 15 feet and a maximum of 30 feet from the building and shall be positioned parallel to one entire side of the building. This will be required to be approved by the fire code official. OFC App D-105.3
11. Required Fire Dept. Access Roads on site shall be designed to support an apparatus weighing 75,000 lb. gross vehicle weight. Provide an engineer's letter stating the access road meets those requirements at time of building permit submittal. OFC, Appendix D, Section D102.1
12. The turning radius for all emergency apparatus roads shall be: 28' inside and 48' outside radius. OFC 503.2.4
13. No Parking Fire Lane signage or curb marking may be required. Fire access roads 20' – 26' wide require the marking on both sides. Indicate on the building permit plans. OFC D 103.6
14. If a gate is installed on a fire access road, it must meet the requirements of the Gresham Fire Gate Policy. OFC 506.1
15. A fire alarm system will be required. OFC 907, 903.4
16. Prior to applying for a building permit provide a fire flow test and report. The fire flow report will verify that the correct fire flow is available and will be required to have been conducted within the last 12 months. OFC 507.3 & B-101.1
17. A KNOX box will be required at the fire sprinkler riser room. If temporary construction gates are used, a KNOX exterior padlock will need to be installed for the duration of its use. These can be purchased at knoxbox.com/greshamfd OFC 506.1
18. Class III standpipe required throughout buildings where the floor level of the highest story is located more than 30 ft above the lowest level of the fire department vehicle access. Shall be installed per NFPA 14. OFC 905.3.1

19. The fire apparatus access shall extend to within 150 feet of all portions of the exterior walls of the first story of the building as measured by an approved route. OFC 503.1.1
20. Fire apparatus access roadway grades shall not exceed 10%. Intersections and turnarounds shall be level (maximum 5%). Where fire apparatus access roadway grades exceed 10% but no more than 12%, buildings are required to have a fire sprinkler system installed. OFC 503.2.7, D103

Multnomah County Transportation Division

1. MCRR 7.000: The applicant must provide a Transportation Impact Study or Assessment that addresses the impact of the development on the level of service and capacity of the following intersections. This needs to be submitted to Multnomah County prior to Building Permit application:
 - i. Cherry Park Rd /Buxton Rd;
 - ii. East Historic Columbia River Highway/Buxton Rd
 - iii. Buxton/2nd
 - iv. Buxton/4th
2. MCRR 8.000 The applicant may be required to provide off-site improvements to mitigate impacts at intersections based on the information provided in the Transportation Impact Study.
3. The applicant is required to contact Mid-County Lighting District (chet.hagen@multco.us) to determine if streetlights are needed. If streetlights are required by the lighting district; this will need to be shown on plans at the time of Building permit application.
4. MCRR 26.000 Applicant must provide a stormwater certificate, and accompanying report with calculations, to Multnomah County (row.permits@multco.us) for review. Review required to ensure that stormwater does not impact County Transportation facilities. Fee for stormwater review \$200.

I. SUMMARY OF PROPOSAL

<u>Applicant:</u>	Leslie Crehan, Project Manager Home Forward 135 SW Ash Street, 5 th Floor Portland, OR 97204 503.802.8463 Leslie.Crehan@homeforward.org
<u>Owner:</u>	Deborah Kafoury, Chair Multnomah County, an Oregon political subdivision 401 N Dixon Street Portland, OR 97227
<u>Architect:</u>	Bill Lanning, Principal MWA Architects Inc 70 NW Couch Street Portland, Oregon 97209 503-348-7388 blanning@mwaarchitects.com
<u>Land Use Planner:</u>	Chris Hagerman, Principal The Bookin Group LLC 1020 SW Taylor St., Suite 500 Portland, Oregon 97205 503.241.2423 hagerman@bookingroup.com
<u>Surveyor:</u>	Troy Tetsuka KPFF Survey 111 SW 5 th Ave., Suite 2400 503.227.3251
<u>Request:</u>	A Type III Site Development Review (Section 7.180) and Type III Variance to Off-Street Parking within the Town Center Overlay for development of a 94-units of affordable housing in three, three-story buildings. A Pre-Application Conference (PRE-0001-2022) was held on February 17, 2022.
<u>Location:</u>	SW Kendall Ct & SW 2 nd St
<u>County:</u>	Multnomah
<u>Property ID:</u>	R240446, R240482
<u>Tax Roll:</u>	Partition Plan 1999-28, Parcel 1 & Partition Plan 1999-13, Parcel 2
<u>State ID:</u>	TL 2501 and 2601, 1N3E25CB
<u>Site Size:</u>	156,014 sf
<u>Zoning:</u>	Central Business District (CBD) with Town Center (TC) overlay

A Type III Site Development Review (Section 7.180) for development of a 94-units of affordable housing in three, three-story buildings. Accessory uses include a community space, and management and resident services offices. Vehicle access to the site will be from SW Kendall Ct. and SW 4th St. Street parking will be provided along the site frontages and 119 spaces within the site. A play area, internal courtyard, and community garden will be provided within extensive landscaping.

Administrative Adjustments are requested to the following design standards:

- Architectural Elements & Building Facades (8.220)
- Outdoor Private Space (8.230)
- Upper Floor Window Standards (A.109)

A Variance is sought for the following:

- Off-Street Parking within the Town Center Overlay District (9.030)

Two additional land use cases for this site will be pursued separately:

- Lot Line Adjustment to eliminate the shared property line between the two parcels to allow them to be developed as a single lot.
- Street Vacation request to vacate the southern ½ of SW 2nd Street between SW Kendall Ct and the terminus of the street at the subject site. The property owner and applicant are currently gathering signatures for the required petition before submitting this review.

Appendices

- A. Pre-Application Conference Notes (PRE-0001-2022)
- B. Wetland Delineation
- C. Trip Generation and Distribution Evaluation
- D. Parking Study

II. EXISTING CONDITIONS

EXISTING SITE CONDITIONS

Vicinity. The Multnomah County-owned site is located in downtown Troutdale immediately east of SW 257th Dr and south of the Historic Columbia River Hwy. A trench containing railroad lines separates the business district from larger commercial and industrial activities to the north (Figure 4). Beaver Creek and the Sandy River are located approximately ¼ mile further east. Housing extends south and east beyond Helen Althaus City Park. Industrial activities predominate east of SW 257th Dr (which becomes Graham Rd north of the site), while commercial and government uses cluster in the immediate vicinity of this vacant property.

The location at the head of the Columbia River Gorge figures into the history and context for the site and provides the variable topography of the area that funnels the east and north winds towards the site.

Site Area. The Site Analysis (Figure 3) shows the current configuration of the site with two existing parcels. A Lot Line Adjustment has been submitted separately to remove the shared lot line between the two parcels that make up the site to make it available for development as a single site.

Site Characteristics. The two properties are presently vacant as shown on the Site Photos (Figures 1 and 2). Parcel 1 is a rectangular corner lot with frontage on unimproved SW 2nd St and SW Kendall Ave, which is improved with a sidewalk, curb, and cul-de-sac. Parcel 2 is an irregular-shaped lot that slopes down to a long, curved frontage along SW 257th Dr on the west side. The site has frontage on SW 4th St to the south, which is improved with paving and a curb, but again steep slopes are present close to the street. Unimproved SW 2nd St terminates at the east property line and provides another 60 feet of frontage. There are mature trees located in the northern reach of the property, at the south property line along the SW 4th St frontage, and along an unnamed perennial stream (Attachment #3) at the southwest corner of the site.

Previous Land Use History. Both parcels were created via the land division process and no other relevant land use history is available for the property.

The single property that will result from the separate Lot Line Adjustment will have the following dimensions:

Proposed Lot Dimensions	
	Proposed Parcel
Area	156,014 sf
Width	380 ft
Depth	609 ft
Front Lot Line(s)	241 ft (SW 4 th St) 471 ft (SW 257 th Dr) 192 ft (SW Kendall Ave) 199 ft (SW 2 nd St)

Land Use Context. The 2020-2040 Town Center Plan adopted by resolution in 2021 considered the site as a part of Opportunity Site 7 – Overlook Tract. The high visibility of the site from 257th Dr was highlighted as needing quality architectural design. The limited visibility from housing to the south provided an opportunity. High Density Residential, Community Service, and Mixed Uses are the preferred future land uses. A possible trail extending from SW 2nd St through the site and crossing SW 257th Dr on a pedestrian and bicycle bridge was identified as a way to improve access for the Foursquare tract across SW 257th from the subject site. This proposed connection was added to the City’s Capital Improvement Plan in May 2022 in anticipation of Home Forward’s forthcoming development.

Zoning. The site is within the Central Business District (CBD) base zone and in the Town Center overlay (TC)

Surrounding Zoning. Properties to the east and north of the site are also zoned CBD. To the south, lots on either side of SW 4th St are zoned Single-Family Residential (R-5) or Apartment Residential (A-2) adjacent to SW 257th Dr. A mixture of Mixed Office Housing (MO/H) and Apartment Residential (A-2) extend to the east and west beyond the CBD zones and General Commercial predominates across the railroad lines to the north.

Existing Transportation System. The characteristics of the streets in the vicinity are shown in Table 1 reproduced from the Preliminary TIA in Appendix X.

Table 1: Roadway Characteristics

Roadway	Average Daily Traffic ¹	Functional Classification ²	Posted Speed Limit	Sidewalks	Transit	Bike Lanes	Lane Geometry	On-Street Parking
Columbia River Highway	9,100	Collector	20 mph	Both sides	Trimet Bus Line #80	Both sides east of Kendall Ave	One 11'-12' lanes in each direction	Both sides between Buxton Road and Kibling Street
SW 257 th Drive	20,500	Arterial	35 mph	Both sides	Trimet Bus Line #81	Both sides	Two 11'-12' lanes in each direction and a center median.	None
SW Kendall Avenue	260	Local Street	25 mph	Both sides	None	None	One ~30' cross-section to accommodate two-way travel and on-street parking.	Both sides
SW 4 th Street	-	Local Street	25 mph	North side, intermittent	None	None	One ~28' cross-section to accommodate two-way travel and on-street parking.	Both sides
Buxton Road	6,770	Collector	25 mph	Both sides	Trimet Bus Line #80	None	One 17'-18' lane in each direction, on-street parking included.	Both sides
SW Cherry Park Road	1,640	Collector	30 mph	Both sides	None	Both sides	One 11'-12' lane in each direction	None

Notes: ¹Based on the PM peak hour count data collected in 2022.

²Based on the City of Troutdale TSP

Traffic Analysis.

A Trip Generation and Distribution Evaluation was prepared by a Transportation Engineer (Appendix C). The existing transportation conditions were evaluated for all modes of travel include pedestrians, bicycles, and motor vehicles.

The following intersections were observed during the PM peak (4:00 pm to 6:00 pm) on February 24, 2022.

- SW 257th Dr at Columbia River Highway
- SW Kendall Avenue at Historic Columbia River Highway
- Buxton Road / S Troutdale Road at SW Cherry Park Road

These observations provide the basis for the analysis presented in the Appendix C and discussed in Chapter III.

Transit Availability. The Multnomah County owned site is directly served by two buses that run on SW 257th Dr – the #77 (Broadway/Halsey) with service between Montgomery Park and Troutdale and the #81 (Kane/257) with service between Troutdale Reynolds Industrial Park and the Gresham Transit Center. Bus #80 (Kane/Troutdale) runs between Gresham Transit Center and Troutdale on Buxton Rd, one block to the east. The #77 runs daily with approximately 30-minute headways. The #81 runs Monday through Friday with approximately the same frequency, while the #80 runs hourly during the day throughout the week.

III. PROPOSED DEVELOPMENT

Proposed development of the site is three, three story, multi-family buildings providing 94 apartments that will be affordable to those at or below 60% of the area median income (AMI). Accessory uses include a community space, and management and resident services offices. Vehicle access to the site will be from SW Kendall Ct. and SW 4th St. Street parking will be provided along the site frontage and 119 spaces within the site. A play area, internal courtyard and community garden will be provided within the landscaped portions of the site.

As a part of the Site Development Review, this proposal includes Administrative Adjustments to the following design standards:

- Architectural Elements & Building Facades (8.220)
- Outdoor Private Space (8.230)
- Upper Floor Window Standards (A1.09.C)

In addition to the Site Development Review, this proposal includes Variances to the following requirements:

- Off-Street Parking within the Town Center Overlay District (9.030)

Public Outreach.

Home Forward convened a Community Advisory Committee that includes adjacent neighbors, social service providers serving east Multnomah County, and low-income residents with lived experience of housing instability. Meetings were held to early site plan concepts in October 2021 and, Home Forward learned about the impact of the Gorge winds in this area and desire for parking close to the units. Based on feedback from the previous CAC meetings, a very different design addressing those concerns and was presented to the Committee in December 2021 and a third meeting was held in April 2022 to present the evolved schematic design for the project.

City Outreach.

Home Forward and MWA have met with the City of Troutdale Planning Department throughout the design process to solicit feedback as follows:

8/23/2021	Early Kick-off meeting with City of Troutdale
10/27/2021	Initial free Pre-AP meeting with City of Troutdale
12/8/2021 to 1/7/2022	4 meetings before the Pre-Application Conference
February 2022	2 Pre-Application Conferences
March 2022	Meeting to discuss the street vacation process with legal staff
April 2022	Home Forward and Metro meeting

2020-2040 Town Center Plan. The proposed use of the site for affordable housing is discussed in this recently adopted plan (2021). As identified in the Minority report section of the Town Center Plan, adding non-residential uses to the project will significantly raise development costs due to prevailing wage requirements. The site is not being developed to its full intensity – the plan envisioned 100-200 units – and while the project will not be limited to only senior citizens, Home Forward’s experience is that a many future residents will likely be seniors and/or people living with disabilities. An on-site Resident Services Manager will connect residents to services and transportation options. The proposal anticipates a trail connection through the site continuing from SW 2nd St and provides a landing space for the proposed pedestrian bridge as shown in Figure 16. This can be accommodated either with or without the successful ½ street vacation of SW 2nd St.

Design Concept

Site Plan Concept

The three L-shaped apartment buildings will step up the hill across the site and are clustered around landscape amenities adjacent to public right-of-way to foster connections to greater Troutdale. A lively streetscape along Kendall Court will feature a full-height glass community room within Building A and an adjacent courtyard that is very active to support families with the main community playground, picnic pavilion and the adjacent community room and resident services offices. A wrap-around covered porch and building transparency encourages flow between inside and outside spaces. The second courtyard acts like a cloister with ample landscaping and more quiet outdoor programming between buildings B and C.

On-site parking is provided in five lots. Dispersing parking across the site helps minimize its impact and shortens distances between the parking and units. The primary site entrance occurs from Kendall Court onto an entry drive populated with landscaped trees and plantings to make an inviting place for cars and pedestrians. A secondary site entry point sits at the top of the hill from SW 4th Street to an upper parking lot adjacent to Building C. The continuous drive aisle provides for fire access and Home Forward is in discussions with the Troutdale Public Works Department to allow emergency use of the driveway if SW 4th were to become impassable.

A robust pedestrian system will provide for circulation around and between the three buildings and the community spaces as well as connecting to the public pedestrian system at the site frontages. A connecting stair alongside Building C allows residents to walk to SW 4th Street. The site is fully accessible between the two courtyards with an internal lift provided off the breezeway in Building C to allow access to the top parking lot.

Massing Concept:

Three 'L'-shaped forms invest the two courtyards with a unique character. The building forms evolved as a response to the Gorge winds and the design minimizes the north exposures for balconies and uses the massing to protect outdoor spaces from the east and north wind.

The massing of Building C along SW 4th St is intentionally limited to two stories to reduce the impact on this residential neighborhood. Because Building C is set lower than the street, residents from across the street will retain Gorge views. A parking lot has been placed in the lower area between the building and the street. This lot will be 3' to 9' lower than the sidewalk along SW 4th St. and trees and landscaping within the 10' setback will screen it from the street.

The simple and repetitive roof forms are inspired by the uncomplicated beauty of terraced hillsides that work with the natural contours of a slope. The roofs are quiet in the east-west long direction and aligned with the natural contours of the site. The north-south legs of the buildings have active gables that contrast the long quiet gables as they step up the hillside.

Each building has one breezeway that flows through from the parking lots to the courtyards to provide site connectivity. An extra-wide and tall breezeway in Building B strengthens the connection between Courtyard B and the lower site. The site mailboxes are located within this breezeway along the drive aisle, adding another place to foster community interaction. All other breezeways are open on one end to limit the wind exposure to these entry points.

Materiality Concept:

The choice of materials aspires to create a high-quality project that contributes to downtown Troutdale. The major siding material is a high-quality fiber cement shingle by Hardie that will be painted for durability. This siding material is higher-quality and more carefully detailed than the lap siding typically found in most housing projects in the region.

The community room is primarily glass storefront with a portion of vertical plank siding on part of the north wall to differentiate this more public function at the base from the housing units above. Large operable windows will maximize daylight and livability for the residents of all unites. In response to the pervasive wind, sliding windows have been specified rather than casement. Most of the living room windows are 7’6” wide x 6’-0” tall, which exceeds the standards in the Troutdale code. Ample daylight supports the wellbeing of the residents in our climate and works well with the scale of the buildings.

All windows are detailed with a protruding metal trim that extends beyond the siding 1” to create crisp shadows that contrast the textured shingle siding. While this does not meet the code requirements for a 1 x 4 flat trim typically seen in residential projects, the intent is for the project to have a modern expression and the depth and shadow of the metal trim creates a richer façade than found with installing flat trim.

Most of the balconies are inset under the main roofs to provide weather protection and improved durability. Painted perforated metal railings at each balcony screen the mini-split heat pumps that provide air conditioning and heat for each unit. Only the studio units with bolt-on balconies will have their HVAC units located on the ground floor, which will be screened by landscaping.

Programming

Building Overviews

Building	Building Area	Height	Total Units	Accessory Uses
A	25,331 sf	46’ 10”	32 units	Community Room, Offices
B	27,010 sf	44’	36 units	
C	22,036 sf	41’ 4”	26 units	
Totals	74,377 sf		94 units	

Unit Mix

Type of Unit	Unit Count	Bdrm Count
Studio	18	18
1-bdrm	29	29
2-bdrm	35	70
3-bdrm	12	36
Totals	94 units	153 bdrms

Expected Population

METRO funding for the Troutdale Development requires 42% of the units (39 units) to house people earning less than 30% MFI (Median Family Income) and 58% of the units (55 units) to house people earning less than 60% MFI. In addition, 10 of the units on the property will be designed as accessible units. The property management team will actively market the property to seniors and those with disabilities. A Resident Services Coordinator will have an office on-site to assist residents.

Landscaping

Design plans for site improvements include a large play area and hardscaped courtyard with seating at the Community Area in building A (~6,000 sf), as well as a large courtyard between buildings B & C (~7,500 sf). There will also be a community garden area (~2,500 sf) at the North Handle portion of the property for residents to use if a parking reduction is granted. An additional recreation area (~1,250) is provided west of Building B.

The project has sought to retain as many mature, healthy trees as possible, in particular those that screen adjacent residential properties from the site. As many trees as practicable will be preserved and incorporated into the landscape plan for the site. The trees close to the unnamed perennial stream delineated in the DSL Wetland Determination (Appendix B) will be protected during development, as will those at the extreme north end of the site, and a large tree shared with a residential property owner to the southeast (Tree Plan L2.03). The tree removal and protection schedule are shown in Tree Legend and Notes (L2.01).

Exterior Lighting. Exterior lighting will match the building exterior. Exterior entry lighting which illuminates the path of egress will be supplied with emergency power to provide illumination in the event of failure. Exterior lighting will utilize full cut off light fixtures to avoid light trespass and will meet associated Dark Sky lighting requirements. Pole mounted fixtures will be at a height of 15 feet and light levels along the property line will be 0.5 footcandles or less.

Transportation

Sight Distance Evaluation

The proposed vehicular access points to the site on to SW Kendall Ave and SW 4th St were evaluated for intersection and stopping sight distances. In the limited cases detailed in Table 2 of the Trip Generation & Trip Distribution Analysis (Appendix C) when the optimum sight distances were not met, the context-specific stopping distances were sufficient and no mitigation was recommended.

Future Conditions

The Trip Generation & Trip Distribution Analysis (Appendix C) created an understanding of current traffic conditions in the vicinity of the site based on observation of the following key intersections:

- SW 257th Drive at Columbia River Highway
- SW Kendall Avenue at Historic Columbia River Highway
- Buxton Road / S Troutdale Road at SW Cherry Park Road

Future Conditions were then predicated based on an expected background growth rate of 2% per year and the addition of predicted site-generated trips based on the proposed use. The estimate of trip generation was based on the institute of Transportation Engineers (ITE) Trip Generation

Manual, 11th Edition category for Affordable House. The 94 units will generate a daily total of 452 new trips per day¹ with only 37 trips during the AM Peak and 43 during the PM peak. This is considerably less than other proposed uses on the site that could be developed by right. Table 4 of the Trip Generation & Trip Distribution Analysis shows two alternative possibilities, a strip retail plaza that would generate 817 total daily trips, nearly double the proposed use, and a Pharmacy/Drug Store with a Drive Through Window, which would generate 1,626 total daily trips, quadrupling the trips generated by the proposed affordable housing development.

Public Improvements. Frontage improvements connected to the proposed residential use for affordable housing for a population that is primarily seniors and/or people living with disabilities are still under discussion with the City of Troutdale to determine what is appropriate for the intensity of the use and cost of development. Three scenarios exist:

- 1) Improvements to SW 2nd St (this allows for an additional 25 parking spaces)
- 2) Vacation of the south ½ of SW 2nd and provision of either:
 - a. Public pathway improvements in SW 2nd St and a connection across the property via an easement and stairs to the public sidewalk at SW 257th Dr.
 - b. Public bicycle and pedestrian path in north ½ of SW 2nd St with a landing space for a future bridge across SW 257th Dr.

The nature of these improvements will be determined through this review and can be issued as conditions of approval subject to appropriate nexus and proportionality findings.

Transit. An on-site Resident Services Coordinator will connect disabled residents to TriMet's LIFT program and elderly and/or disabled residents will be connected to Ride Share. As discussed in Chapter II, transit service is available on SW 257th Dr and on Buxton Rd.

Parking. This proposal includes 119 on-site parking spaces and 11 on-street parking spaces along the project frontages, resulting in a ratio of 1.38 spaces per unit. This is consistent with the needs of the expected population in the housing and the observations of both other Home Forward properties as well as other affordable housing sites. The Parking Study provided by the Transportation Engineer in Appendix D documents that the demand for parking in affordable housing properties in east Multnomah County is 1.09 spaces per unit or 103 spaces for 94 units of affordable housing. Observations at two similar affordable housing properties in another suburban location (Beaverton), that was not near light rail and served only by one bus line, identified an overnight parking demand of only 0.74 spaces per unit.

While a mixed-use proposal would with the same unit count would not have required parking, this residential-only proposal triggers a requirement for 2 spaces per unit and therefore a Variance is sought to reduce the amount of required parking.

Approximately half of the units will be either studio or 1 bedroom and will most likely serve seniors and/or people living with disabilities. As discussed in Chapter II, the observations of similar projects indicate that 77% of the studio or 1-bedroom units in affordable housing are occupied by seniors and/or people living with disabilities. OF the 94 units on site, 47 units on the proposed site will be studio or 1-bedrooms, suggesting that approximately 36 will be occupied by persons that will not generate the same demand for on-site parking. Additionally, 10 units on the property will be designed as accessible units and the property management team will actively market the

¹ This is below the 1000 Average Daily Trips that would generate the need for a full TIA per Troutdale Administrative Rules 003 (TAR003).

property to seniors and those with disabilities. Trimet's Lift and Ride Share programs provide transportation for people who are unable to drive, further reducing the need for parking on site.

Additional parking could be provided at the northern end of the site, but this area has been reserved for a community garden and a potential pedestrian bridge landing location across 257th.

Bicycle Parking

10 short-term bicycle parking spaces will be provided on the site. Two bicycle racks will be placed at the north end of Building C and 3 will be placed at the frontage on SW Kendall Ave adjacent to the sidewalk, the playground, and the community room. 8 long-term bicycle parking spaces will be provided in a designated bike storage room in Building C that contains 4 racks (Sheet A2.08).

IV. LEGAL FINDINGS

Request. A Type III Site Development Review (Section 7.180) for development of 94-units of affordable housing in three, three-story buildings. Accessory uses include a community space, and management and resident services offices. Vehicle access to the site will be from SW Kendall Ct and SW 4th St. Street parking will be provided along the site frontages and 119 on-site spaces. A play area, internal courtyard and community garden will be provided within extensive site landscaping.

As a part of the Site Development Review, this proposal includes Administrative Adjustments to the following design standards:

- Architectural Elements & Building Facades (8.220)
 - A. All three buildings have elevations that exceed 40' in length
 - D. The side elevation of Building B does not meet the window requirement
 - F. All three buildings exceed the 40' offset standard for rooflines
- Outdoor Private Space (8.230)
 - A. 5 studios do not have private outdoor space
 - B. Ground floor units have a 42-inch screen and plantings
- Upper Floor Window Standards (A1.09)
 - C. The dimension of the upper-story windows for residential units exceeds the maximum size of the standard.

In addition to the Site Development Review, this proposal includes Variances to the following requirements:

- Off-Street Parking within the Town Center Overlay District. (9.030)

Related Reviews

Additional land use cases for this site will be pursued separately:

- Lot Line Adjustment (Section 7.180) to eliminate the shared property line between the two parcels to allow them to be developed as a single lot.
- Street Vacation (Section 6.1200) request to vacate the southern ½ of SW 2nd Street between SW Kendall Ct and the terminus of the street at the subject site. The property owner and applicant are currently gathering signatures for the required petition before submitting this review.

A Pre-Application Conference (PRE-0001-2022) was held on February 17, 2022. Meeting notes are contained in Appendix A.

Legal Findings

As shown below, the proposed parcel meets the site and design standards as well as the approval criteria for a Site Development Review with adjustments and a variance.

Chapter 3 – Zoning Districts

3.130 Central Business District

3.134 Dimensional Standards.

A. Lot Area, Lot Width, and Lot Depth.

1. **Non-residential and mixed use: No minimum requirement.**

Findings: The site is proposed to be developed for residential use; therefore, there are no minimum requirements for the lot dimensional standards.

2. Residential uses:

- a. Minimum lot area shall be based on the minimum lot width and minimum lot depth standards. Where there is no minimum lot width or minimum lot depth required, there shall be no minimum lot area requirement.
- b. Residential uses shall have the following dimensional standards:
 - i. Minimum lot width: Sixteen (16) feet.
 - ii. Minimum lot depth:
 - (a) Seventy (70) feet for residential units with a driveway from the public street or with access from an alley within a separate tract from the lot.
 - (b) Ninety (90) feet for residential units with access from an alley within an easement that is part of the lot.

Findings: The lot is significantly larger than 90 feet in depth; therefore, if developed with a residential use, the proposed lot will meet these standards.

- (c) There is no minimum lot depth for lots within the area between Historic Columbia River Highway and 2nd Street extended west to its intersection with 257th Avenue from 257th Avenue to the SE Sandy Street right-of-way.

Findings: The site is only partially within the area between Historic Columbia River Highway and SW 2nd St east of 257th Ave; therefore, the minimum lot depth standards of (b) and (c) above apply to the proposed lot. As demonstrated above, the proposed lot can meet those standards.

B. Street Frontage: Minimum of sixteen (16) feet.

Findings: The site has more than 16 feet of frontage; therefore, this standard is met.

C. Setbacks:

2. Residential uses:

a. Front yard setback:

- i. Without alley access:
 - (a) Minimum of twenty (20) feet to the garage door of residential units with a driveway from the public street.
 - (b) Minimum of fifteen (15) feet to the front façade of a residential unit.
 - (c) Minimum of ten (10) feet to the front porch of a residential unit.

Findings: The relevant front yard setbacks are 15 feet to the front façade and 10 feet to the front porch. All residential buildings and porches are set back at least 15 feet from the right-of-way.

b. Side yard setback:

- i. Adjoining a non-residential or A-2 zoning district: Minimum of five (5) feet.
- ii. Adjoining a residential zoning district other than A-2:
 - (a) Single-story construction: Minimum of five (5) feet from an adjoining side yard and a minimum of fifteen (15) feet from an adjoining rear yard.
 - (b) Two-story or greater construction: Minimum of seven and one half (7-1/2) feet from an adjoining side yard and a minimum of fifteen (15) feet from an adjoining rear yard.
 - (c) The minimum side yard setback from an adjoining rear yard may be reduced as provided in Section 3.137 of this Chapter.
- iii. No side yard setback for interior side property lines of duplexes, triplexes, and attached dwellings on individual lots.

Findings: The relevant side yard setbacks are 5 feet to the non-residential property lines and 7.5 feet and 15 feet to the residential property located at the southwest corner of the site.

d. Rear yard setback:

- i. Adjoining a non-residential zoning district:*
 - (a) Without an alley: Minimum of ten (10) feet.*
- ii. Adjoining a residential district:*
 - (a) Without an alley:*
 - (i) Single-story construction: Minimum of fifteen (15) feet.*
 - (ii) Two-story and greater construction: Minimum of twenty (20) feet.*
 - (iii) The minimum rear yard setback may be reduced as provided in Section 3.137 of this Chapter.*

Findings: The relevant rear yard setbacks would be 10 feet to the non-residential property lines and 20 feet to the residential property located at the southwest corner of the site; however, as discussed in Chapter III, the site is a corner lot with two additional non-intersecting frontages. SW Kendall Ct is clearly the front lot line, therefore, the lot line abutting SW 257th is the property line directly opposite the front lot line. However, the definition of Rear Lot Line (1.020.80) specially defines it as a line that does not abut a street. Therefore, the site does not have a rear lot line and no rear yard setbacks apply.

D. Residential Density.

- 1. Maximum residential density when the dwellings are all on one (1) lot shall be one (1) dwelling unit per one thousand five hundred (1,500) square feet of net land area, otherwise the maximum density shall be determined on the basis of the minimum lot area standards as established in Subsection (A)(2) of this Section.*

Findings: The site size is 156,014 sf, resulting in a maximum density of 104 units (104 = 156,014 / 1,500). The proposal is for 94 units and complies this standard.

- 2. For the area between Historic Columbia River Highway and 2nd Street extended west to its intersection with 257th Avenue from 257th Avenue to the SE Sandy Street right-of-way, the maximum residential density when the dwellings are all on one (1) lot shall be one (1) dwelling unit per 1,000 square feet of net land area. There is no maximum residential density for units on individual lots within this area.*

Findings: The northern portion of the site is within this area, but no housing is proposed in this area.

- 3. For exclusively residential developments, residential development is required to be built at eighty percent (80%) or more of the maximum number of dwelling units per net acre.*

Findings: Given a maximum density of 104 units, the minimum residential development for an exclusively residential development is 83 units. The proposal exceeds this minimum and therefore, is in compliance with this standard.

- 4. Apartment units built in conjunction with a commercial use are not subject to the above maximum and minimum density standards.*

Findings: At this time the proposal does not include a commercial use, but if one is added no minimum or maximum density standards would apply.

E. Height Limitation. The maximum height of a structure shall be thirty-five (35) feet.

Findings: As described in Chapter III, the three multi-dwelling buildings are 38', 36'-9", and 32', respectively. The unique topography of the site often results in the base of the buildings being positioned below the grade of the surrounding streets minimizing the visual effect of three-story buildings. While the proposed heights exceed the maximum height of the base zone, the proposal makes use of additional height available for affordable housing projects under Senate Bill 8 passed by the State Legislature in 2021. Affordable housing meeting the requirements of the bill are allowed a density of 150% of existing maximum density and up to 24 ft of additional height. For this proposal, the additional height required is 3' for Building A and 1'-9" for Building B. With consideration of Senate Bill 8, this standard is met for the proposal.

3.136 Additional Requirements.

A. *Access and Circulation. Adequate provisions for access and internal circulation of vehicles shall be provided for all uses allowed in the CBD in accordance with the requirements of this Code.*

Findings: The site has two access points and a linked series of parking lots that allow for internal circulation of vehicles within the site. This requirement is met.

B. *External Storage of Merchandise. The external storage of merchandise and/or materials, directly or indirectly related to a business, is hereby prohibited within the CBD.*

C. *Outdoor Displays of Merchandise. Outdoor displays of merchandise are permitted during business hours only and shall not exceed ten percent (10%) of the total retail sales area.*

Findings: The proposed development is for residential use; therefore, the additional requirements of B and C of this section do not apply.

D. *Off-Street Parking and Loading.*

1. *No off-street parking and loading shall be required for non-residential uses.*

2. *A minimum of two (2) parking spaces per unit is required for residential uses, except that dwelling units in conjunction with commercial uses are required to have a minimum of one (1) parking space per dwelling unit.*

Findings: 119 on-site and 11 on-street spaces are proposed for the 94 residential units. This is less than the minimum two (2) parking spaces per unit and a Variance to this standard is requested to reduce the minimum parking to 199 spaces (a ratio to 1.38 spaces per unit) due to the lower parking needs for affordable housing that contains a substantial portion of older and/or disabled residents as well as the desires of the City to maintain a landing area for a proposed bike-ped bridge across SW 257th Dr at SW 2nd St.

3. *When parking is provided, the parking shall conform to the standards of Chapter 9, Off-Street Parking and Loading, of this Code. When conflicts exist between this Section and Chapter 9, Off-Street Parking and Loading, of this Code, this Section shall apply.*

Findings: The requirements of this section are discussed below and demonstrated to be met with the exception of 9.030, for which a variance is requested below.

E. *Unless otherwise provided in this Code, compliance with Chapters 8 and 11 relating to design review and landscaping is required. The Design Standards for Central Business District, listed in Appendix A of this Code, shall also apply to the CBD.*

Findings: As discussed below, the proposal complies with Chapters 8 and 11 or an Adjustment or Variance is sought.

F. *Town Center Overlay District. The applicable provisions of Chapter 4.600, Town Center, of this Code shall apply to the CBD.*

Findings: The applicable provisions of the Town Center overlay are discussed below.

4.600 TOWN CENTER TC

4.605 Applicability. *The regulations and standards of this overlay district apply to land within the boundaries of the Town Center Planning as established in the Town Center Plan except they shall not apply to those properties designated Low-Density Residential/Open Space in the Plan.*

Findings: The site is within the boundaries of the Town Center Plan as shown on the Zoning Map; therefore, the provisions of this section apply.

4.630 Town Center Residential Densities.

A. *General Density Requirements. The residential density of the underlying zone shall apply except that the Central Business District (CBD) density standards shall apply in the CC and GC zoning districts and shall apply in the A-2 zoning district for duplex, triplex, and attached residential developments.*

B. *Minimum Density. Residential development is required to be built at eighty percent (80%) or more of the maximum number of dwelling units per net acre.*

Findings: This site is zoned CBD and the relevant density standards have been discussed above. These standards are met.

4.640 Dimensional Standards. *Dimensional standards shall be the same as those listed in the underlying zone...*

Findings: This site is zoned CBD and the relevant density dimensional standards have been discussed above. These standards are met.

4.660 Residential Design Review. *All residential development other than detached single-family, zero lot line, and duplex dwellings on separate lots shall be subject to site and design review and design standards specified in Chapter 8.200 of this Code.*

Findings: The proposal for residential development is subject to these standards, which are discussed below.

4.680 Street Design and Streetscapes. *The following design standards shall apply within the Town Center district:*

A. *Blocks and Access. The perimeter of blocks shall not exceed fifteen hundred (1,500) feet. Blocks along arterial and collector streets shall be designed to allow streets to intersect in a manner that allows the side yards of development to abut the arterial or collector street. In general, development should not be designed with rear yards abutting arterials and collectors.*

Findings: The site is within a block that exceeds the 1,500-foot standard of 4.680.A; however, there is not an opportunity to increase connectivity through the site given the existing development on the remainder of the block, the topography, and the inability to take gain access to SW 257th Dr.

B. *Street Termination. Unless impractical due to efficiency of street layout and design, topography, or other site constraints of the property being developed, new street sections shall be no longer than twelve hundred (1,200) feet without providing a jog, a deflected view, traffic island, or a point of termination, such as a "T" intersection.*

Findings: No new street sections are required or proposed; therefore, this standard does not apply.

C. *Streetscapes. To encourage pedestrian-oriented streetscapes, the following standards shall apply:*

1. *Fences and walls greater than three and one half (3 ½) feet in height shall be prohibited in front yards. If fences or walls greater than three and one-half (3 ½) feet in height are provided along street side yards or rear yards abutting streets, the fence shall be buffered from the public right-of-way by a landscaped strip no less than five (5) feet wide.*

Findings: No fences or walls are proposed in the front yard of the frontage on SW Kendall Ct. The other frontages on the site function as side lot lines and this limitation on wall height does not apply. This standard is met.

3. *Street trees are required along public streets in accordance with the City's Tree Ordinance, Troutdale Municipal Code, Chapter 13.10.*

Findings: This standard will be met at the time of development.

4. *Local residential streets shall have a pavement width of twenty-eight (28) feet, with sidewalks set back and separated from the street by a planting strip of five (5) feet in width. The street shall provide on-street parking on both sides of the street.*

Findings: The site has frontage on two local streets, SW Kendall Ct and SW 4th St, both of which have at least 28-feet of paving, curb-tight sidewalks, and a planter strip. This proposal maintains the existing conditions of the right-of-way and is not moving either further of compliance.

4.690 Off-Street Parking and Loading. *Off-street parking and loading shall be provided in accordance with the requirements of the underlying zoning district and with Section 8.225, Off-Street Parking, Garages, and Carports, and Chapter 9, Off-Street Parking and Loading, of this Code as applicable. Except for residential units on individual lots, no use shall be permitted to exceed the required minimum amount of off-street parking by more than ten percent (10%);*

Findings: The proposal is for less than the minimum off-street parking as required by Chapter 9 and a variance is requested below. The standards of Section 8.225 and Chapter 9 are met as shown on the Civil Drawings (C1.00 – C3.00).

4.695 Authority to Adjust Standards.

A. *Because of the diverse topography and parcel configurations within the TC district, it is neither practical nor feasible to uniformly apply these design standards to all development projects. The Director shall use reasonable discretion in determining whether the standards in Sections 4.650-4.680 of this Chapter are practical for individual developments. The Director is authorized to grant administrative adjustments to these design standards upon making the following written findings:*

1. *The adjustment is justified due to unique site conditions.*
2. *The proposal will be consistent with the desired character of the area.*
3. *Any impacts from the adjustment are mitigated to the extent practical.*

B. *When, in the Director's opinion, an adjustment to a design standard is not justified, the request shall be handled as a variance in accordance with the procedures of Chapter 6.1300, Variance,*

of this Code. The Directors decision to adjust specific standards is a Type II decision under Section 2.055, Type II Procedure, of this Code.

Findings: Adjustments to 4.650, if necessary, will be considered under this authority. For this site, the topography is particularly challenging with significant differences between the center of the site and the adjacent right-of-ways, particularly to the south and west. The apartment proposal is an appropriate transition between the single-dwelling residential uses to the south and the more intensely developed Town Center areas to the north and east. The high-quality design, abundant landscaping, and detailed pedestrian areas will mitigate for any impacts from not meeting any of the applicable standards.

Chapter 5, Misc. Uses, Standards, and Exceptions

5.040 Clear Vision Areas.

- A. A clear vision area shall be maintained as provided below.
- B. The clear vision area is a triangle that is measured according to the following:
 - 1. A clear vision area at intersections of local streets with local streets shall consist of a triangular area, two (2) sides of which are the curb lines extended in a straight line to a point of intersection and so measured as defined and illustrated in Figure 5.040(A), and the third side of which is a line joining the non-intersecting ends of the other two sides. Where no curb exists, the edge of street pavement shall be used in lieu of the curb line.
 - 2. Driveways and alleys intersecting with local public streets shall have and maintain a clear vision triangle with its base measured along the face of curb parallel to the public way for one hundred ten (110) feet in both directions from the center of the driveway, and the other sides extending toward the apex of the triangle in the center of the driveway fifteen (15) feet from the street curb line (see Figure 5.040(B)). Where no curb exists, the edge of street pavement shall be used in lieu of the curb line.
 - 3. The clear vision area for local streets, alleys, and driveways intersecting with a county or state road shall comply with jurisdictional standards.
 - 4. Alternatively, a clear vision triangle may be established by a site- specific analysis conducted by an Oregon Design Professional in accordance with the standards set forth in the most current edition of “A Policy on Geometric Design of Highways and Streets” published by the American Association of State Highway and Transportation Officials.

Findings: As shown on Sheet A1.00 and discussed by the project transportation engineer in the Trip Generation and Distribution Evaluation, the site distances at the intersection and proposed driveways are sufficient.

- C. Except as provided below, within clear vision areas, no vehicle, fence, wall, hedge, or other planting or structure (temporary or permanent) shall be parked, erected, planted, placed, located, or maintained above three (3) feet in height measured from the top of the curb or, where no curb exists, from the established street centerline grade of the intersecting streets and from the driveway centerline at a driveway intersection, except for occasional tree trunks, mail boxes, street sign posts, or utility poles, so as to impede visibility within the clear vision area.

Findings: As show in the landscape sheets (L1.00—L1.03), the vision area will remain clear as specified in these standards.

5.050 Fences, Walls and Windscreens.

- A. *Fences and retaining walls on lots zoned residential shall comply with the following:*

1. *The clear vision standards of Section 5.040 of this Code.*
2. *Not exceed four (4) feet in height if located in a required front yard setback, except the height limit shall be six (6) feet for a dwelling that existed prior to June 9, 1987 and which fronts a major arterial.*
3. *Not exceed seven (7) feet in height if located in a required side or rear yard setback area.*
4. *Fences and retaining walls shall be constructed of wood, brick, masonry cement, chain link, plastic, wrought iron or similar residential-type materials. The use of barbed wire, electric fences, sheet metal or other non-residential materials is prohibited.*

Findings: As discussed in Chapter III and shown on Sheets C2.00 and G3.01—G3.03, there are several retaining walls proposed due to the complicated topography of the site. These vary in height, but in most cases are invisible from the public right-of-way or adjacent properties. The site sections demonstrate that these are outside of the setbacks. These standards are met.

- *Fences, retaining walls on lots zoned commercial or industrial shall comply with the following:*
 1. *The clear vision standards of Section 5.040 of this Code.*
 2. *Not exceed three and one-half (3 1/2) feet in height if located in a required front yard setback,*
 3. *Not exceed six (6) feet in height if located in a required side or rear yard setback area.*

Findings: The CBD base zone is included in the Commercial/Industrial category. The clear vision standards of Section 5.040 are met. As discussed in Chapter III and shown on Sheets C2.00 and G3.01—G3.03, there are retaining walls that exceed six (6) feet in height, but they are outside of required side or rear yard setbacks as discussed above. These standards are met.

5.300 Nonconforming Uses.

Findings: As the site is vacant, there is no non-conforming development on the site that is subject to this section. The site is also not nonconforming in area or dimension.

5.700 Stormwater Management

5.730 Applicability. No land use action shall be approved which does not make adequate provisions for stormwater or floodwater runoff. The stormwater drainage system shall be separate and independent of any sanitary sewer system. Water quality treatment for stormwater is required as indicated in the City's Construction Standards for Public Works Facilities.

Findings: The project civil engineer has developed a preliminary plan to manage stormwater separately from the sanitary sewer system on the development portions of the site as shown in the civil plans (C1.00—C3.00).

Chapter 6 – Applications

6.900 SITE DEVELOPMENT REVIEW

6.905 Purpose. The purpose of this Chapter is to advance all of the following objectives in the public interest:

- A. *Carry out the development pattern and plan of the City through efficient and effective review of site development proposals;*

- B. Promote the public health, safety and general welfare;
- C. Provide adequate light and air, prevent overcrowding of land, and provide for adequate transportation, water supply, sewage, fire protection, pollution control, surface water management, and protection against natural hazards; and
- D. Encourage efficient use of land resources and public services, and provision of transportation options.

6.910 Applicability and Exemptions. Site development review approval is required for new development...

Finding: Site Development review is required for this proposal for new development and none of the exemptions apply.

6.915 Review Procedures. Site development review shall be conducted using a Type I or Type II procedure to be determined as follows:

- B. A Type II application shall be used to review all of the following:
 - 5. Adjustment to applicable design standards for any development proposal.

Findings: As there are adjustments to design standards requested for this development proposal, it will be processed as a Type II application.

6.920 Approval Criteria. In order to approve a site development review application, the decision making authority shall make findings of fact based on evidence provided by the applicant demonstrating that the proposal is consistent with the applicable approval criteria.

A. An application for a Type I site development review shall be approved if the proposal meets all of the following criteria. The City decision-making body may, in approving the application, impose reasonable conditions of approval, consistent with the applicable criteria.

- 1. The application complies with all of the applicable provisions of the underlying zone and overlay zone(s), including but not limited to: building and yard setbacks, lot area and dimensions, density and floor area, lot coverage, building height, building orientation, architecture, and other applicable standards, or a variance or adjustment is granted.

Findings: As discussed above and detailed throughout this application, the proposal complies with the listed standards with the exception of elements for which an adjustment to a design standard is sought, or regulations for which a variance will be requested. This criterion is met.

- 2. The proposal includes required upgrades, if any, to existing development that does not comply with the applicable land use district standards, pursuant to Section 5.300 Nonconforming Uses.

Findings: As discussed above, the site does not have an existing use or development that is subject to Section 5.300. The site is also not a nonconforming lot subject to 5.350. Therefore, this criterion does not apply.

- 3. The proposal complies with all of the applicable site design and development standards of this Code, such as landscaping and parking.

Findings: As discussed above and detailed throughout this application, the proposal complies with the applicable standards for landscaping and parking with the exception of elements for which an adjustment to a design standard is sought, or regulations for which a variance will be requested. This criterion is met.

- 4. The proposal meets all existing conditions of approval for the site or use, as required by prior land use decision(s), as applicable.

Findings: *There is no relevant land use for the site; therefore, there are no existing conditions of approval that remain in effect.*

B. *An application for a Type II site development review shall be approved if the proposal meets all of the following criteria. The City decision-making body may, in approving the application, impose reasonable conditions of approval, consistent with the applicable criteria.*

1. *The proposal complies with the approval criteria specified in Section 6.920.A.*

Findings: *As discussed above and detailed throughout this application, the proposal complies with the approval criteria with the exception of elements for which an adjustment to a design standard is sought, or regulations for which a variance will be requested*

2. *If applicable design standards are proposed to be adjusted, the proposed adjustment:*

a. *Is justified due to unique site conditions.*

b. *Conforms to the extent practicable with these design standards.*

c. *Mitigates potential impacts from the adjustment to the extent practical*

Findings: *The following adjustments to design standards are sought for this proposal:*

#1. 8.220 Architectural Elements & Building Facades. A, D, and F

#2 8.230 Outdoor Private Space. A and B

#3 A1.09.C Upper Floor Window Standards. C

In addition, a Type III Variance is requested for 9.030 Off-Street Parking within the Town Center Overlay District to reduce the required parking of the site from 177 to 119, a decrease of 33%. This request is discussed below.

6.1300 VARIANCE

6.1305 General Provisions.

A. The variance procedures are intended to allow modifications of specific standards contained within this Code when authorized as provided below.

B. In approving a variance request, the approving authority may attach reasonable conditions, restrictions, or safeguards to mitigate any adverse impacts which may result by reason of the approved variance.

C. Separate variance provisions apply to uses within the Vegetation Corridor and Slope District and the Flood Management Area.

D. Because variance applications arise from unique situations, the decision making authority will consider each application on its own facts and merits. Variances are discretionary, prior variances allowed within the City do not establish a precedent or control present or future variance applications.

Findings: *These provisions are informational.*

6.1310 Regulations Which May Not Be Varied.

A. No variance may be granted which will permit a use not permitted in the applicable zoning district.

B. No variance may be granted which will increase the maximum residential density or decrease the minimum residential density allowed in the applicable zoning district.

C. No variance may be granted to the provisions of Chapter 5.300, Nonconforming Uses and Developments of this Code.

Findings: The request for variances for this proposal is neither for a not-permitted use, nor an increase to the maximum residential density, nor for a provision of the nonconforming uses and developments section. This regulation is met.

6.1315 Type I Variance. The Director may grant a variance under the Type I procedure if the request involves the expansion or reduction of a quantifiable provision in this Code by no more than ten percent (10%), and the following criteria are met:

- A. Special circumstances or conditions including, but not limited to, lot size, lot shape, topography, or size or shape of building, apply to the property, development, or to the intended use and are not typical of the general conditions in the surrounding area; and*
- B. The variance authorized will not be injurious to adjacent properties or the surrounding neighborhood or otherwise detrimental to the public welfare; and*
- C. The variance is the minimum necessary to relieve a practical difficulty with full compliance and to avoid or minimize the resulting hardship.*
- D. The variance authorized will be consistent with the general purpose and intent of the provision from which a variance is sought; and*

Findings: The minimum parking per 9.030, is 188 spaces (2 per unit), which then is reduced per 9.040 by 11 spaces provided by on off-site street frontages, resulting in 177 required spaces. This variance request is to further reduce the parking requirement by 33%, a reduction of 58 spaces to reflect the actual parking needs of the population that this affordable housing will serve, the proximity to transit, and the other public needs for the site.

This proposal includes 119 on-site parking spaces and 11 on-street parking spaces along the project frontages, resulting in a ratio of 1.38 spaces per unit. This is consistent with the needs of the expected population in the housing and the observations of both other Home Forward properties as well as other affordable housing sites. The Parking Study provided by the Transportation Engineer in Appendix D documents that the demand for parking in affordable housing properties in east Multnomah County is 1.09 spaces per unit or 103 spaces for 94 units of affordable housing. Observations at affordable housing in another suburban location (Beaverton) identified an overnight parking demand of only 0.74 spaces per unit.

The population to be served by this housing is primarily seniors and/or people living with disabilities. As discussed in Chapter II and III, the observations of similar projects indicate that 77% of the studio or 1-bedroom units in affordable housing are occupied by seniors and/or people living with disabilities. 46 of the 84 units on the proposed site will be studio or 1-bedrooms, suggesting that approximately 35 will be occupied by persons that will not generate the same demand for on-site parking. Additionally, 10 units on the property will be designed as accessible units and the property management team will actively market the property to seniors and those with disabilities. Trimet's Lift and Ride Share programs provide transportation for people who are unable to drive, further reducing the need for parking on site.

While a mixed-use proposal with the same unit count would have required parking of only 1 space per unit, this residential-only proposal triggers a requirement for 2 spaces per unit and therefore a Variance is sought to reduce the amount of required parking by 33%. The percent reduction is higher because Home Forward has agreed to work cooperatively with the City to accommodate its desire for a pedestrian bridge in this location. While the City is still in the early planning stages for the proposed bridge, Home Forward has agreed to proceed with a

site design that would not preclude development of a pedestrian bridge if/when the City secures the necessary approvals and funding.

Additional parking could be provided at the northern end of the site (See Option 3 on Figure 16), but this area has been reserved for a community garden and the potential pedestrian bridge landing location across 257th to serve the City's goals in creating pedestrian and bicycle connectivity to other properties. The 2020-2040 Town Center Plan, completed in 2021, identified a potential bike-ped bridge across SW 257th Dr to connect a trail through the Foursquare Tract (Site 6) on the west side of 257th to downtown via 2nd St. The 2nd St Pedestrian Bridge project as describe below was added to the Capital Improvement Project (CIP) in March 2022 (Resolution 2331) to take advantage of "opportunities for partnerships with other agencies and stakeholders to construct projects sooner than later" and "the limited timeframe for capital project [sic] to be considered due to adjacent development".

A Second Steet Bridge – a 14-foot wide bike/pedestrian bridge that crosses over 257th Drive and would link downtown Troutdale to the future food cart pod and a future multi-use path along the Halsey Street corridor to connect with Edgefield, Wood Village, and Fairview.

The site has a difficult topography and shape as shown on the A1.00 and C2.00 plans that requires extensive grading and retaining walls to maintain flat spaces for buildings, parking lots, and driveways. The immediate primary neighbors are open space, an arterial road (SW 257th Dr) and public buildings (County Sheriff's office, etc.). The parking needs for housing peaks at night, which is the inverse of businesses and public offices. The reduction in required parking by 33% brings the required parking in-line with the observed parking ratios of similar projects in suburban locations and also maintains the viability of the possible pedestrian connection for SW 2nd St through the site and the opportunity to provide a community garden in the northern portion of the site.

The criteria for a variance are met by this proposal.

6.1320 Type II Variance. The Director may grant a variance under the Type II procedure if the request involves the expansion or reduction of a quantifiable provision in this Code by more than ten percent (10%), but not more than thirty percent (30%), and the criteria in Section 6.1315 are met.

6.1325 Type III Variance. The Planning Commission may grant a variance under the Type III procedure if the request involves the expansion or reduction of a quantifiable provision in this Code by more than thirty percent (30%), or if the request is referred to the Planning Commission in accordance with Section 6.1335 of this Chapter. The variance shall be granted only if the Planning Commission determines that the criteria in Section 6.1315 of this Chapter are met

Findings: The variance request is a reduction of 58 spaces from the minimum required 177 (after reduction per 9.040 from 188). Absent proposed pedestrian 2nd St bridge, an additional parking lot could be provided (See Option 3 on Figure 16) and the requested reduction would be only 23 spaces or 13% -- a Type II Variance. The reduction in the minimum parking standard will better meet the need to the residents and due to the need to preserve a landing in the "handle" for the 2nd St ped-bike bridge proposed by the City. Therefore, the request will be processed as a Type III Variance reviewed concurrently with this application. As discussed above, the criteria of 6.1315 of this Chapter are met by the proposal.

6.1335 Referral to Planning Commission. The Director may refer any variance request involving the expansion or reduction of a quantifiable provision of this Code by thirty percent (30%) or less to the Planning Commission if the Director determines that a higher level of review is justified given the complexity or controversial nature of the request. A variance that is referred to the Planning Commission shall be considered in accordance with Section 6.1325 of this Chapter.

Chapter 8 – Site Orientation and Design Standards

8.000 SITE AND DESIGN STANDARDS

8.010 Purpose. The purpose of this chapter is to advance all of the following objectives in the public interest:

- A. Carry out the development pattern and plan of the City and its comprehensive plan policies through efficient and effective review of site development proposals;
- B. Promote the public health, safety and general welfare;
- C. Provide adequate light and air, prevent overcrowding of land, and provide for adequate transportation, water supply, sewage, fire protection, pollution control, surface water management, and protection against natural hazards; and
- D. Encourage efficient use of land resources and public services, and provision of transportation options.

8.020 Applicability and Exemptions. Site development review approval is required for new development...

Findings: This repeats the requirement that a site development review is necessary for new development.

8.030 Pedestrian Walkways. In addition to the standards above, all industrial parks, commercial developments, and community service uses shall meet the following requirements for pedestrian walkways:

Findings: This proposal is for residential uses only; therefore, these standards do not apply.

8.040 Transit Facility Design. Any Type II land divisions where further divisions are possible, and all Type III land divisions, multiple-family developments, community services uses, and commercial or industrial uses located on an existing or future transit route shall meet the requirements of TriMet for transit facilities. Applicants shall consult with TriMet to determine necessary transit facility improvements in conjunction with the proposed development. Proposals shall be consistent with the road crossing improvements that are identified in the City Transportation System Plan on streets with existing or planned transit service.

Findings: The applicant has been in consultation with TriMet.

8.045 Additional Transit Related Design Standards. All commercial and community service development...

Finding: The proposed use is for only residential use; therefore, these design standards do not apply.

8.200 MULTIPLE-FAMILY, ATTACHED, DUPLEX, AND TRIPLEX DWELLING DESIGN STANDARDS

8.205 Purpose. The purpose of this Section is to provide general design standards for residential dwellings other than single-family detached, zero lot line dwellings, and duplex dwellings on

separate lots. This Chapter is intended to promote and ensure that high quality architectural building designs for these types of residential developments are maintained throughout the City

8.210 Applicability. In addition to the development standards of the underlying zoning district or overlay zoning district and applicable Sections of Chapter 8, Site Orientation and Design Standards, of this Code, the provisions of this Section apply to all residential dwellings other than single-family detached, zero lot line dwellings, and duplex dwellings on separate lots. This Section also does not apply to dwelling units built above, below, or behind the street level floor of a commercial use.

8.215 General Design Standards.

A. Building Entrances. The primary entrance to buildings that do not have residential units above or below other residential units shall face toward a public or private street, unless the lot configuration, site characteristics, onsite circulation, or other conditions make it impractical to face a building's primary entrance to the street. Exception: A primary entrance is not required to face a street designated as an arterial.

Findings: The three buildings proposed for apartments all have residential units above and below other residential units; therefore, the building entrances standards do not apply.

B. Building Separation. Multiple residential buildings on a single lot shall be separated at least fifteen (15) feet from one another.

Findings: Building A is located 83' from Building B, which is at the closest point to Building C, is separated by 17'-3½" as shown on Sheet A1.00. This standard is met.

C. Pedestrian Access and Circulation.

1. Private streets or driveways greater than twenty (20) feet in length and that serve more than one (1) dwelling unit shall have sidewalks on at least one (1) side that connect to the nearest public street.

2. Each primary entrance to a residential building shall be connected to a sidewalk onsite that connects to either a public street, private street, or driveway.

3. Onsite pedestrian circulation shall be continuous and connect streets abutting the site, ground level entrances, common buildings such as laundry and recreation facilities, parking areas, shared open space and play areas, abutting transit stops, and any pedestrian amenities such as plazas, resting areas, and viewpoints. There shall be at least one (1) pedestrian connection to an abutting street frontage for each two hundred (200) linear feet of street frontage.

4. Vehicle/pedestrian conflicts shall be minimized by providing pedestrian routes that are separated from parking lots, including onsite sidewalks that connect to garbage enclosures or recycling areas and mailboxes.

Findings: There is a complete pedestrian network within the site that serves all buildings, garbage enclosures, mailboxes, and parking areas. The driveways to the site all have at least one sidewalk that connects to the nearest public street and all the building entrances connect to a sidewalk that connects to a street. These standards are met.

8.220 Architectural Elements and Building Facades. Residential units shall be designed with vertical and horizontal offsets to break up rooflines, define private outdoor areas, allow greater views, and admit light and air to unit interiors. Large, blank walls shall be avoided. Windows and projecting walls shall be used to break up larger walls in order to establish visual interest.

A. No wall of a residential building shall exceed a length of forty (40) feet without a foundation offset of at least four (4) feet for a distance of at least sixteen (16) feet.

Findings: While the buildings are highly articulated with entrances, balconies and windows, façades on each of the three buildings exceed the 40' standard or the offset is less than 16'. The key façades are as follows:

- Building A: south and east (elevations #2 and #3 on Sheet A4.00)
- Building B: south and north (elevations #3 and #4 on Sheet A4.02 and #1 of A4.03)
- Building C: east and north (elevations #1 and #4 on Sheet A4.04)

The building footprints are shown on Sheet A1.00 and are dimensioned on the elevations in Sheets A4.01—A4.05. Perspective views of the façades are provided in Sheets G2.00—G2.02. In most cases, these long façades are at the end of building forms and face side lot lines. Where they face public areas, entrances, articulation in rooflines, balconies, and sequences of windows meet the intent of the standard in avoiding large, blank walls and an adjustment to this design standard is requested in 8.240, below. Additionally, the other façades of the buildings provide offsets deeper than (4) feet and more frequently than (40) feet which present a highly articulated project that breaks up the scale of the buildings.

B. The wall of a building that faces a public street shall incorporate architectural features including, but not limited to, at least three (3) of the following:

1. *Porches.*
2. *Balconies.*
3. *Dormer windows.*
4. *Recesses/alcoves.*
5. *Unique entry areas, such as porticos or atriums.*

Findings: The building orientations are shown on Sheet A1.00 and elevations are found in Sheets A4.01—A4.05. Perspective views of the façades are provided in Sheets G2.00—G2.02. Each of the façades that faces a street meets the standards as follows:

- Building A - the north façade (SW 2nd St) contains recesses, porches, and balconies.
- Building A – the east façade (SW Kendall Ct) contains recesses, balconies, and a unique entry.
- Building A – the west façade (SW 257th Dr) contains recesses, balconies, and unique entries.
- Building B – the west façade (SW 257th Dr) contains balconies, recesses, and porches.
- Building C – the south façade (SW 4th St) contains recesses, balconies, and a unique entry

These standards are met.

C. The same level of architectural design and quality of materials shall be applied to all sides of the building.

Findings: As seen in the elevations (A4.00-A4.04), the perspective drawings (G2.00—G2.02) and the views (Figures 3—13), the same level of architectural design and quality of materials has been applied to all sides of the three buildings. A sheet with images of selected materials will be provided during the review. This standard is met.

D. The following window detail shall be incorporated into the building design:

1. *Windows shall account for at least fifteen percent (15%) of any rear or front building elevation no matter what the building's orientation on the lot is.*

2. *Windows shall account for at least ten percent (10%) of any side building elevation no matter what the building's orientation is.*

Findings: As seen in the elevations (A4.00—A4.04) and Window Schedule (A6.00—A6.01), the standards for window detail are met on nearly all the facades. However, the south elevation of Bldg. B (a side building elevation) does not meet this standard as it does not have windows. This elevation faces the closest neighbor to the project, so windows are oriented away from their view. This wall is setback 31'-9" from the south property line and 132' from SW 4th St and there is ample landscaping in this setback that will screen and buffer this wall. A modification to this standard is requested above.

3. *All windows shall have outer casings or frames.*

Findings: All windows are framed with a 1" metal trim that will cast a deep shadow on the elevation and provide depth to the elevation. To contrast this simple, sharp shadow line, the shingle siding is highly textured, more so than traditional lap siding, with wide flat trim. Shingled siding relates to the context as documented in the thorough site analysis that preceded the design phase, which included visually mapping the existing buildings and material textures in downtown Troutdale (see Figures 8 and 9). This standard is met.

- E. *Garages, carports, and accessory structures shall maintain the same level of design, aesthetic quality, and architectural compatibility as the residential structure(s).*

Findings: The trash enclosures are detailed in Sheet A1.50 and show a similar level of detailing and design as the residential buildings that is appropriate to their utilitarian nature and low visibility on the site.

- F. *Roofline offsets shall be provided at intervals of forty (40) feet or less to create variety in the massing of structures and relieve the effect of a single, long roof. Roofline offsets shall be a minimum four (4) foot variation either vertically from the gutter line, or horizontally.*

Findings: Similar to the building forms in section A above, the roof lines do not meet the numerical standard of offsets every 40' but the rooflines are articulated in a manner that meets the intent of the standard to create variety in the massing of the structure and avoid the effect of a single, long roof. Having more quiet, long roof eaves on the north and south edges provides a balanced contrast with the end-gable elevations that are highly articulated and exceed the offset the requirements. Further, the wall elevations with the long roof eaves provide insets that are 5' deep every 26' (on average), therefore exceeding the wall articulation requirement of section A8.220.A. This is demonstrated in the elevations (A4.00-A4.04), the perspective drawings (G2.00—G2.02) and the views (Figures 3—13). An Adjustment to this standard is requested as a part of this application.

- G. *The rooflines of attached dwellings shall be multi-gabled or have varied parapets over every unit's main entrance.*

Findings: This is demonstrated in the elevations (A4.00-A4.04), the perspective drawings (G2.00—G2.02) and the views (Figures 3—13). This standard is met.

- H. *Rooflines, porches, and doors shall have trim.*

Findings: The Door / Window schedules (A6.00—A6.01) and Exterior Detail Sheets (A8.00—A8.32) demonstrate this required detailing. This standard is met.

8.225 Off-Street Parking, Garages, and Carports. In addition to the standards of Chapter 9, Off-Street Parking and Loading, off-street parking for attached dwellings on a single lot shall include these design standards:

- A. *Parking lots may not be located between a multiple-family structure and the right-of-way the structure fronts on.*

Findings: The parking lots are not located between any of the buildings and the front lot line on SW Kendall Ct. The other lot lines that abut streets function as side lot lines and do not provide frontage. Therefore, all the parking lots are in compliance with this standard. Parking Lot C is located between SW 4th St and Building C but it is well below the adjacent sidewalk in grade and landscaping will obscure it from view. See site sections on sheet G3.02.

- B. *If there is no parking lot or alley access...*

Findings: Does not apply since parking lots are provided.

- C. *Side and rear yard setbacks for parking lots and/or garages shall be the same as the minimum building setbacks of the underlying zoning district or the same as the minimum building setback of any adjacent residential zoning district, whichever is more restrictive. Side and rear yard setbacks based on building height shall not be applied to parking lots or the access driveways.*

Findings: As shown on Sheet A1.00 and discussed earlier in this Chapter, the parking lots remain outside of the minimum building setbacks as determined by the more restricted adjacent residential zoning. This standard is met.

8.230 Outdoor Private Space. Outdoor private space is required for each residential dwelling unit.

- A. *Each ground level dwelling unit shall have an attached accessible outdoor private space of not less than sixty (60) square feet in area. Individual outdoor areas for ground level units must be visually screened from each other by walls, fences, or vegetation screening that is at least six (6) feet high and totally sight-obscuring.*

Findings: The ground floor units have 50 sf individual outdoor areas that are delineated by a 42-inch perforated metal screen. Landscape plantings are provided in front of this screen for increased privacy. An adjustment to this standard is requested in 8.240, below.

- B. *Each upper level unit shall have an attached outdoor private space, such as a balcony, of not less than sixty (60) square feet in area. The area shall be enclosed, screened, or otherwise designed to provide privacy from adjacent units by walls, building offsets, or similar sight-obscuring screening.*

Findings: The private patios for upper-level units are 50 sf and are recessed to provide privacy. This meets the intent, but not the numerical standard. Additionally, five studios above the community room or on facades with only inset balconies, do not have any outdoor private space. An adjustment to this design standard is sought in 8.240, below.

8.235 Recreation Areas.

- A. *Recreational facilities or open space areas are required for attached dwelling developments that contain six (6) or more dwelling units on one (1) lot. Such recreational facilities and open space areas must be located on the development site or on a lot adjacent to the site. Common recreation areas, whether indoor, outdoor, or both, shall be provided at the rate of at least two hundred (200) square feet per dwelling unit. No more*

than fifty percent (50%) of the required recreation area may be in passive open space. Recreation facilities may include children's play structures and play equipment and shall be located outside of bioswales, detention ponds, steep slopes, or a vegetation corridor as defined in this Code. More than one (1) recreation area may be developed on the site. Any play structure exceeding ten (10) feet in height must comply with the underlying zoning district setbacks.

Findings: Attached dwelling in the Troutdale Development Code refers narrowly to single-family development with common vertical walls (1.020.46). Multi-family apartments are a separate category. Therefore, this standard does not apply to the proposal. The site does contain abundant recreation and open space areas that account for more than 20,000 sf of area and include playgrounds, active recreational uses, and community gardens.

8.240 Authority to Adjust Standards.

A. *Because of the diverse topography, parcel configurations, and site characteristics within the City, it is neither practical nor feasible to uniformly apply these design standards to all development projects. The Director may grant adjustments to these design standards upon making the following written findings:*

1. *The adjustment is justified due to the unique site conditions.*
2. *The proposal conforms to the extent practicable with these design standards.*
3. *Any impacts from the adjustments are mitigated to the extent practical.*
4. *The decision to adjust a standard is a Type II decision.*

Findings: The applicant requests adjustments to the following design standards:

- 8.220.A. Residential Building Wall length
- 8.220.D. Window Detail
- 8.220.F. Roofline Offsets
- 8.230.A Private Space Ground Floors
- 8.230.B. Private Space Upper Floors
- A109.C. Upper Story Window Standard

8.220.A. Residential Building Wall length

8.220.F Roofline Offsets

The building wall length and roofline offsets will be considered together as they stem from the design decisions regarding building form and the desire to orient the buildings to protect courtyards from the Gorge winds. The massing concept is to contrast quiet roofs with active roofs, creating a balanced and composed design.

The simple and repetitive roof forms are inspired by the uncomplicated beauty of terraced hillsides that work with the natural contours of a slope. The roofs are quiet in the east-west long direction and aligned with the natural contours of the site. The north-south elevations of the buildings have active and repetitive gables that contrast the long quiet gables as they step up the hillside. Additionally, the walls with the long quiet gables exceed the articulation requirements with deep balconies and breezeways. See Figures 11, 12 and 13 for diagrams that explain these design concepts.

Additionally, the unique topography of the site screens many of the building facades from view. The articulation of the buildings and placement on the oddly-shaped site with four street frontages mitigates the long facades and rooflines to the extent practicable by using additional detailing and architectural elements including recesses, unique entries, balconies, porches, and wide breezeways to break up the building form.

8.220.D. Window Detail

The south elevation of Bldg. B (a side building elevation) does not meet this standard as it does not have windows. This elevation faces the closest neighbor to the project, so windows are oriented away from their view. This wall is setback 31'-9" from the south property line and 132' from SW 4th St and there is ample landscaping in this setback that will screen and buffer this wall. Site Section #3 on Sheet G.3.01 shows the distance and elevation between the neighboring residential structure and the Building B façade. The combination of topography and vegetation will mitigate for the lack of windows on this façade.

8.230.A Private Space Ground Floors

8.230.B. Private Space Upper Floors

The ground floor private space will be screened with a 42-inch perforated metal pattern that is 18-inches shorter than the 60-inch standard, but the screens combined with landscaping in front of the units will provide the same level of privacy.

The majority of the units have private outdoor space. There are five studio units that do not have an outdoor balcony because of their location above the community room or on a façade with only inset balconies. The project presents a cohesive whole to the community of Troutdale by maintaining a clear design concept. Bolt-on balconies have been added to the studio units located on the south elevation of Bldg. A and the north elevation of Bldg. C because it was conceptually consistent and appropriate to add them at these tall elevations. Adding bolt-on balconies to the 5 studios that are inside the main building massing would disrupt the composition of the elevations.

The balconies are 50 sf, smaller than the 60 sf required by the standard. For an affordable housing project, the provision of small and efficient units is fundamental to meeting community housing needs. Provision of full-sized private outdoor space for every unit is impractical architecturally for this project and would undermine the affordability of the units. Abundant interior community space and exterior spaces are provided that will mitigate for this elimination of a private balcony. Connecting individuals to the community of residents in these areas outweighs the need for private outdoor space for these studio units.

A.109.C Upper-story window standard

In addition, an adjustment to the Upper-story window standard found in Appendix A (A.109.C) is requested. The standard seeks to limit the size of upper-story windows, requires 2-inches of trim, and encourages breaking up large windows into smaller panes. As shown on Sheets A4.00—A4.03 and A6.00—A6.01, the upper floor windows of the residential units are larger than the standard by 18 inches to provide more day light and views for residents. The proportion of these windows is appropriate for the scale of the buildings and presents an opening and welcome façade to the community. Smaller windows are less gracious in this façade.

The trim for the window is a 1-inch protrusion instead of a 2-inch trim to add shadow and depth to create a rich façade. The shingle siding was inspired by textures found in downtown Troutdale. The crisp metal trim contrasts this highly textured siding and will provide shadow lines that a flat trim would not. This elevates the quality of the façade beyond that typically seen in residential projects.

Chapter 9, Off-Street Parking and Loading

9.005 Off-Street Parking Required. Off-street parking and loading space shall be provided for all developments....

Findings: Multiple-family dwelling (apartments) have a minimum of 2 spaces per dwelling plus one space per three dwelling units for guest parking. There is no maximum.

9.010 Residential Off-Street Parking Space Requirements.

Findings: The minimum off-street parking space requirements are as follows: 94 units x 2 spaces = 188.

9.030 Off-Street Parking within the Town Center Overlay District.

A. *No minimum off-street parking spaces are required for non-residential uses in the Central Business District (CBD) or Mixed Office/Housing (MO/H) zoning districts. Within these zones, a minimum of two (2) parking spaces per unit is required for residential uses, except that residential units in conjunction with commercial uses are required to have a minimum of one (1) parking space per residential unit.*

Findings: The site is within the Town Center Overlay District; therefore, the minimum parking is two (2) parking spaces per unit for residential uses or one (1) in conjunction with commercial uses. For 94 units, this would result in a minimum required parking of 188 spaces.

B. *Except for residential units on individual lots, no use within the Town Center Overlay District shall be permitted to exceed the required minimum amount of off-street parking by more than ten percent (10%); however, each use shall be allowed at least one (1) parking space in excess of the minimum amount required.*

Findings: This proposal does not seek to exceed the minimum. However, if a commercial use was added to the proposal, the required minimum parking would drop to 94 spaces and the number of spaces currently proposed (119) would need to be reduced to meet this standard.

9.040 Reduction of Required Parking Spaces.

A. *Any existing or proposed use subject to minimum off-street parking requirements and located within four hundred feet (400) feet of an existing transit route may reduce the number of required parking spaces by up to ten percent (10%) by providing a transit stop and related amenities including a public plaza, pedestrian sitting areas, or additional landscaping, provided such landscaping does not exceed twenty-five percent (25%) of the total area dedicated for transit-oriented uses.*

B. *Required parking spaces may be reduced at a ratio of one (1) parking space for each one hundred (100) square feet of transit amenity space provided above and beyond the minimum required by this Code.*

Findings: These provisions provide a reduction in requirement parking for sites with transit access. The site is adjacent to an existing transit route and therefore may reduce the number of required parking spaces if a transit stop is provided.

- C. *Required off-street parking spaces may be reduced by one (1) parking space for every on-street parallel parking space located adjacent to the subject site. For purposes of calculating the amount of adjacent on-street parking spaces, the following applies:*
1. *Adjacent shall mean on the same side and within the same block as the use.*
 2. *The minimum length of each on-street, parallel parking space shall be twenty-two (22) feet.*
 3. *If a continuous section being measured contains a fractional portion of twenty-two (22) feet, then the number of on-street spaces for that continuous section shall be rounded down to the next lower whole number.*
 4. *Breaks in the on-street parking for driveways or similar parking restrictions, such as fire hydrants, shall not be counted.*
- Findings:** 11 off-street parking spaces that meet these standards are proposed to be provided on SW 4th St and SW Kendall Ct, reducing the required parking by an equal amount, resulting in a minimum required on-site parking requirement of 173 spaces.
- D. *Uses which are not eligible for these reductions include truck stops, building materials and lumber sales, nurseries, and similar uses not likely to be visited by pedestrians or transit customers.*
- Findings:** As a use likely to be visited by pedestrians and transit customers, the proposed residential use is eligible for these reductions.

9.45 *Landscaping and Screening.*

- A. *Except for a residential development which has landscaped yards, parking areas containing more than twenty (20) vehicle spaces shall include landscaping to cover not less than ten percent (10%) of the area devoted to parking facilities. The landscaping shall be uniformly distributed throughout the parking area and may consist of trees, shrubs, or groundcover.*
- Finding:** The proposal is for a residential development with extensively landscaped yards; therefore, this standard does not apply.
- B. *Parking areas shall be divided into bays of not more than twenty (20) parking spaces in parking areas with twenty (20) or more spaces. Between, and at the end of each parking bay, there shall be planters which have a minimum width of five (5) feet and be at least seventeen (17) feet in length. Each planter shall contain one major structural tree and groundcover which has been deemed appropriate by the Director. Truck parking and loading areas are exempt from this requirement.*
- C. *Parking area setbacks shall be landscaped with major trees, shrubs, and groundcover as specified in Chapter 11, Landscaping and Screening, of this Code.*
- D. *Wheel stops, bumper guards, or other methods to protect landscaped areas shall be utilized. No vehicles may project over a property line. No vehicle shall overhang a public right-of-way, sidewalk, or landscaped area unless adequate area is provided for safe pedestrian circulation.*
- Finding:** As shown on C2.00 and G2.01, none of the parking areas have bays that exceed 20 spaces. The landscaping plans, L1.01—L1.03 and L3.01—L3.03, show the required planters. The necessary wheel stops will be provided. These standards are or will be met.
- E. *Fences, walls, or hedges shall not be placed within front or street side setback areas except at the street side edge of parking lots when allowed within setbacks.*
- Findings:** The proposed lots are in compliance with this standard. As shown on Sheet C3.00, Lot C has a retaining wall at the street-side edge; however, it is located beyond the required

setback and is hidden from street view because the parking lot is below the elevation of the right-of-way.

- F. *Where parking adjoins a residential zoning district, there shall be a sight-obscuring screen which is at least eighty percent (80%) opaque when viewed horizontally from between two (2) and eight (8) feet above average ground level. The screening shall be composed of materials which are an adequate size so as to achieve the required degree of screening within three (3) years after installation.*

Finding: The landscaping plans, L1.01—L1.03 and L3.01—L3.03, show the required plantings are shown in the setbacks adjoining residentially zoned properties that will meet this standard.

- 9.50 Paving. **Findings:** the parking areas and approaches will be paved to City standards. These standards will be met.

9.055 Drainage. *Parking areas, aisles, and turnarounds shall have provisions made for the on-site collection of drainage waters to eliminate sheet flow of such waters onto sidewalks, public right-of-ways, and abutting private property*

Findings: As shown on the C series Civil Plans (C2.00—C3.00), the proposed grading of the parking lots provides for the collection of drainage waters. Stormwater facilities are detailed in sheet C3.00. These standards are met.

9.060 Lighting. *Artificial lighting shall be provided in all required off-street parking areas. Lighting shall be deflected so as not to shine directly into adjoining dwellings or other types of living units and so as not to create a hazard to the public use of a street. Lighting shall be provided in a bicycle parking area so that all facilities are thoroughly illuminated and visible from adjacent sidewalks or motor vehicle parking lots during all hours of use. Lighting fixtures shall also comply with the requirements of Troutdale Municipal Code, Chapter 8.26.*

Findings: Lighting will be provided as discussed in Chapter III to illuminate parking areas, bicycle parking and entrances. These lights will be shielded to avoid spill over into other properties. These standards will be met.

9.70 Driveways.

- A. *A driveway to an off-street parking area shall be improved from the public roadway to the parking area a minimum width of twenty (20) feet for a two-way drive or twelve (12) feet for a one-way drive, but in either case not less than the full width of the approach for the first twenty (20) feet of the driveway. The improvement shall be constructed to the standards for private drives.* **Findings:** There are two access points to the site. Each will be improved to the standards for private drives.
- B. *A driveway for a single-family or two-family dwelling shall have a minimum width of ten (10) feet.* **Findings:** The proposal is for multi-dwelling residential use. This standard does not apply.
- C. *Driveways, aisles, turnaround areas, and ramps shall have a minimum vertical clearance of twelve (12) feet for their entire length and width, but such clearance may be reduced in parking structures.* **Findings:** This vertical clearance standard will be maintained. This standard will be met.
- D. *Parking lots more than three (3) acres in size intended for use by the general public shall provide street-like features along driveways, including curbs, sidewalks, street trees or planting*

strips, and bicycle routes. **Findings:** The proposal does not include parking lots larger than three acres. This standard does not apply.

0.075 *On-Site Circulation.* **Findings:** As shown on Sheet C3.00, all parking spaces will be permanently marked, and adequate aisles or turnaround areas provided so that vehicles may enter the street in a forward manner. The on-site circulation standards are met.

9.080 *Bicycle Parking Facilities.* *Multiple-family developments; industrial, commercial and community service uses; transit transfer stations; and park and ride lots, shall meet the following standards for bicycle parking facilities:*

A. Number/Type.

- 1. The required minimum number of short-term bicycle parking spaces (stays of less than four (4) hours) shall be five percent (5%) of the total number of automobile parking spaces provided for the use. In no case shall less than one (1) bicycle parking space be provided even when no automobile parking spaces are being provided.*
- 2. The required number of long-term bicycle parking spaces (stays of more than four (4) hours and all-day/monthly) shall be three percent (3%) of the total number of vehicle parking spaces provided for the use and fractions rounded down.*

Findings: Minimum short-term bicycle parking for the proposal is 9 spaces (5% * 188 required spaces). Minimum long-term bicycle parking is 5 (3% * 188 required spaces).

B. Location.

- 1. Bicycle parking shall be located on-site, convenient to building entrances, and have direct access to both the public right-of-way and to the main entrance of the principal use.*
- 2. For facilities with multiple buildings or parking lots, bicycle parking shall be located in areas of greatest use and convenience to bicyclists.*

C. Parking Space Dimensions. *Each required bicycle parking space shall be at least two and one half (2.5) feet-by-six (6) feet, and when covered, provide vertical clearance of at least seven (7) feet. An access aisle of at least five (5) feet wide shall be provided and maintained beside or between each row of bicycle parking. Vertical or upright bicycle storage structures are exempted from the parking space length standard.*

D. Parking Facilities. *Bicycle parking facilities shall offer security. Long-term bicycle parking shall be in the form of a lockable enclosure, a designated bicycle storage area inside a building on-site, a covered rack, or another form of secure parking where the bicycle can be stored, as approved by the Director. Short-term bicycle parking shall be in the form of a stationary object (i.e., a "rack") or other approved structure, covered or uncovered, to which the bicycle can be locked. Bicycle racks shall be securely anchored to the ground or to a structure and shall be designed to hold bicycles securely by means of the frame. Bicycle parking facilities shall be constructed so as to not obstruct walkways.*

E. Signing. *Where bicycle parking facilities are not directly visible and obvious from the public right-of-way, entry and directional signs shall be provided to direct bicyclists from the public right-of-way to the bicycle parking facility.*

Findings: Bicycle parking that meets these standards is shown on Sheet L1.01. Two bicycle racks will be placed at the north end of Building C and 3 will be placed at the frontage on SW Kendall Ct adjacent to the sidewalk, the playground, and the community room. Long-term bicycle parking will be provided in a designated bike storage room in Building C that contains 4 racks (Sheet A2.08). If deemed necessary, signage will be provided. These standards are met.

9.085 Setbacks.

- A. *Parking areas which abut a residential zoning district shall meet the building setback of the most restrictive adjoining residential zoning district.*

Findings: The southwest corner of the site abuts R-5 zoning on TL 3100 and TL3200 (201 and 225 SW 4th St). In the R-5 zone, the building setback is 10'. Parking Lots B and C are each set back more than 15' from the shared property line (Sheet A1.00). Therefore, these standards are met.

- B. *Required parking shall not be located in a required front or side yard setback area abutting a public street except in industrial districts. For single-family dwellings, required parking may be located in front of a garage.*

- D. *Parking areas shall be set back from a lot line adjoining a street the same distance as required building setbacks. Regardless of other provisions, a minimum setback of ten (10) feet shall be provided along the property fronting on a public street in an industrial district. The setback area shall be landscaped as provided in this Code.*

Findings: Parking Lot C is located greater than 10' from the street lot line on SW 4th St. The building is setback 84'-6" from SW 4th, and the side yard setback is 24'-0" to the east, respectively. Parking Lot B and E take access from the bulb of the cul-de-sac on SW Kendall Ct, but each of the parking areas are set back 10' from the right-of-way (Sheet A1.00). Therefore, these standards are met.

9.110 Design Requirements for Off-Street Parking. *The following off-street parking development and maintenance shall apply in all cases:*

Findings: As shown on Sheet A1.00 and the C series Civil Plans, the proposed layouts of the parking lots meet the dimensional standards of this section.

9.120 Off-Street Parking Plan. *A plan drawn to scale, indicating how the off-street parking and loading requirement is to be provided...*

Findings: As shown on Sheet A1.00 and the C series Civil Plans (C2.00—C3.00), the proposed layouts of the parking lots meet the dimensional standards of this section. Stormwater facilities are detailed in sheet C3.00. These standards are met.

Chapter 11 – Landscaping and Screening

11.010 Minimum Basic Improvements. *These standards apply to developments other than single-family detached and duplex dwelling units on a single lot.*

- A. *The minimum area of a site to be retained in landscaping shall be as follows:*

CBD – Central Business District 5%

Findings: As shown on the landscape plans (L1.00—L1.03), much of the site is landscaped well in excess of the 5% minimum in the zone. This standard is met.

- B. *For attached dwellings, including mixed-use development, usable recreation areas shall be provided for development containing more than five (5) dwelling units at the rate of two hundred (200) square feet per dwelling unit. Such areas shall be counted as part of the required landscaping percentage. Examples include, but are not limited to, playgrounds, exercise trails, swimming pools, etc.*

Findings: As discussed above, this proposal is for multi-family apartment dwelling; therefore, this standard does not apply.

- C. *Except for portions approved for parking, loading, or traffic maneuvering, a required setback area abutting a public street, and open area between the property line and the roadway in the public street, shall be landscaped. This landscaping shall be counted as part of the required landscaping percentage, except for that portion of the landscaping within the street right-of-way.*

Findings: As shown on the landscaping plans, Figures 14-15 and Sheets L1.01—L1.03, the required areas will be landscaped. This standard is met.

- D. *Site-obscuring shrubbery or a berm, wall, or fence shall be placed along the boundary of each classification of zone, i.e. residential, commercial or industrial, and around unsightly areas such as a trash or equipment storage area, or an outdoor industrial or commercial activity.*

Findings: The southwest corner of the site borders properties residentially-zoned properties that are elevated above the grade of the proposed site. As shown on the sections on Sheet G3.01, the setback and an additional 8' will be landscaped with sight obscuring plantings behind a fence that will limit the view of the development to only the top floor of the adjacent buildings. Similar plantings will be placed around the trash enclosures. This standard is met.

- E. *Landscaping that is required by a land use approval shall be irrigated to ensure the survivability of the landscaping.*

- F. *At least seventy-five percent (75%) of the required landscaped area shall be planted with a suitable combination of trees, shrubs, or evergreen groundcover.*

- G. *Plant Material:*

- H. *Landscaped areas may include architectural features or artificial groundcovers such as sculptures, benches, masonry or stone walls, fences, rock groupings, bark dust (medium coarse), decorative hard paving, and gravel areas, interspersed with planted areas. The exposed area developed with such features shall not exceed twenty-five percent (25%) of the required landscaped area. Artificial plants are prohibited in any required landscaped area.*

Findings: These standards will be met at the time of development.

- I. *Existing trees with a six (6) inch DBH or greater shall be preserved except when removal is specifically authorized by the Site and Design Review Committee or in the development approval.*

Findings: The tree schedule and plans in Sheets L2.01—L2.03 show the trees proposed for removal to allow proposed development or because they are either an invasive species or are dead, dying, or dangerous as determined by the project landscape architect. L2.02 This standard will be met.

- J. *The area of the vegetation corridor on a site being developed counts toward the required landscape area.*

Findings: The site does not contain a Vegetation Corridor; therefore, this does not apply.

11.015 Garbage and Recycling Container Enclosures. *All enclosures used to contain garbage and recycling containers at multiple-family, commercial, industrial, or institutional developments must conform to the following minimum standards:*

- A. *Screening. All enclosures for garbage and recycling containers must be screened from public view. Screening shall consist of six (6) foot high walls constructed of any of the following materials:*

1. *Cyclone fencing with slats.*
2. *Wooden fencing.*

3. Concrete blocks.
 4. Materials other than the above-mentioned as approved on a case-by-case basis.
- B. Gates. Gates must meet the following requirements:
1. Must have a latch or some type of device which will keep the gate shut after it is closed. The device can be above or below ground.
 2. Must have a mechanism to keep gates open during trash removal. The device can be above or below ground.
 3. Wheels are not required; however, the hinge must be adequate to support the weight of the gate.
- C. Base Material/Flooring. The entire base dimension must meet the following requirements:
1. Must be made out of concrete. Concrete shall have a nominal thickness of four (4) inches. Exceptions to the base materials may be approved by the Director where warranted.
 2. Must be positively sloped to the drainage system.

Findings: As shown on Sheets A1.50, the garbage and recycling enclosures have been designed to these specifications including: walls that are 6-feet high made of concrete blocks and chain-link with privacy slats, mechanized gates, and concrete floors sloped to the drainage system. These standards are met.

Appendix A – Design Standards for Central Business District

A.101 Purpose.

- A. The purpose of these design standards is to guide the design of buildings constructed in the CBD zoning district to ensure that, through appropriate use of facades, windows, building orientation, and architectural details, new structures and alterations of existing structures are physically and visually compatible with other buildings within the downtown business district. Existing buildings in the downtown area reflect architectural styles that were popular during the early to mid-twentieth century. It is the desire of the City to have buildings conform to architectural styles of this era. The design standards are intended to further define those characteristics that cause buildings to look like they were constructed during this period.
- B. These standards are intended to encourage good quality design in new building construction, enhance street safety, and provide a comfortable street environment by providing features of interest to pedestrians. Good design results in buildings that are in visual harmony with nearby buildings, leading to a central downtown district that is attractive, interesting, active, and safe. These qualities, in turn, contribute to the creation of a downtown core which facilitates easy pedestrian movement and establishment of a rich mixture of uses.

A.102 Dimensional Standards.

- A. Lot Area, Lot Width, and Lot Depth: As specified in the CBD zoning district.
- B. Setbacks: As specified in the CBD zoning district.
- C. Landscaping: Minimum five percent of site area, except that no minimum landscaping is required for the area between Historic Columbia River Highway and 2nd Street extended west to its intersection with 257th Avenue from 257th Avenue to the SE Sandy Street right-of-way.

Findings: As discussed above, the site meets the dimensional standards of the CBD zoning district, the proposed development meets the setback requirements, and the landscaping exceeds the minimum 5%. These standards are met.

D. Building Height: The maximum height of a structure fronting on Historic Columbia River Highway shall be measured above the grade of Historic Columbia River Highway as it occurs adjacent to the lot.

Findings: The site does not have frontage on the Historic Columbia River Highway; therefore, this standard does not apply.

A.103 Pedestrian Accessibility.

- A. Buildings shall maintain and enhance the pedestrian scale and orientation of the downtown core.*
- B. Building entries must comply with the accessibility requirements of the Oregon State Structural Specialty Code.*
- C. Special attention shall be given to designing a primary building entrance which is both attractive and functional.*
- D. Buildings located at the intersection of two (2) streets shall consider the use of a corner entrance to the building.*
- E. Pedestrian environment may be enhanced by street furniture, landscaping, awnings, and movable planters of seasonal flowers.*

Findings: The buildings have been articulated and are below surrounding grade to limit their sense of scale and bulk when seen from the street. While located at the corner of SW 2nd St and SW Kendall Ct, it was determined that the primary entrance to Building A will be from the plaza accessed from SW Kendall Ct, which features site furniture, an overhead canopy, and large-format pavers (L1.03). Buildings B and C are entirely residential with individual unit entries, but generous breezeways will function as primary entrances into the complex (L1.00—L1.02). These standards are met.

A.104 Compatibility with Existing Downtown Businesses. The size and shape of proposed construction shall be comparable with the size and shape of nearby traditional storefront buildings. Where building sizes will not be equivalent or comparable, larger building facades shall be broken down into units that resemble the size of existing storefront facades. Likewise, the form of new construction shall complement the general shape of existing, nearby storefront buildings and their features.

Findings: As shown in the context figures, there are a variety of storefront forms and material treatments in the vicinity. However, there are few traditional storefront buildings directly adjacent to the location of the proposed development. The public facing facades of the proposed apartments are broken down into smaller units, scaled to be compatible with the forms of traditional storefronts. The focal point of the development is from the corner of SW Kendall Ct. and SW 2nd St and the generous storefront window coverage of the Community Room on the first floor of Building A captures this aesthetic. This standard is met.

A.105 Building Materials and Colors.

- A. Facades shall be varied and articulated to provide visual interest to pedestrians. Within larger projects, variations in facades, floor levels, architectural features, and exterior finishes shall create the appearance of several smaller buildings.*

- B. *Exterior building materials shall convey an impression of durability. Materials such as masonry, stone, stucco, and wood are encouraged. Metal is not allowed as the primary exterior building material, but it may be used for accents including awnings.*
- C. *Where masonry is used for exterior finish, decorative patterns must be incorporated. Examples of these decorative patterns include multicolored masonry units such as brick, stone, or cast stone, in layered or geometric patterns, or split-faced concrete block to simulate a rusticated stone-type construction.*
- D. *Wood siding must be bevel, shingle siding, or channel siding and must not be applied in a diagonal or herringbone pattern. T1-11 style siding is not permitted.*
- E. *Preferred colors for exterior building finishes are earth tones, creams, and pastels of earth tones. High-intensity primary colors, metallic colors, and black may be utilized as trim and detail colors but shall not be used as primary wall colors.*

Findings: As shown on Sheets A4.00—A4.05 and Figures 20—30, the primary exterior material will be a high-quality, articulated fiber cement board and batten siding. Colors have not been selected but will come from the prescribed list of options. The facades of the three buildings have been articulated to provide visual interest through a varied roof line, building massing that takes advantage of the site’s unique topography, and placement of windows, balconies, and breezeways. These standards have been met.

A.106 Roof Materials, Parapets, and Roof Pitch.

- A. *Pitched roof structures shall have a minimum roof pitch of 6:12.*
- B. *Flat roofs are permitted with detailed stepped parapets or detailed brick coursing.*
- C. *Parapet corners must be stepped or the parapet must be designed to emphasize the center or primary entrance(s), unless the primary entrance is at the corner of the building.*
- D. *Visible sloped roofs must be a “non-color”: gray, black, or dark brown.*
- E. *Visible roof materials must be wood or architectural grade composition shingle, or sheet metal with standing or batten seam.*
- F. *All roof and wall-mounted mechanical, electrical, communications, and service equipment, including satellite dishes and vent pipes, must be screened from public view by parapets, walls, or by other approved means.*

Findings: Sheets A5.00—A5.05 show the roof pitches and the roof plans area shown in Sheet A2.03, A2.07, and A2.11 demonstrating that these standards have been met.

A.107 Building Orientation and Entrance Standards.

- A. *Buildings must have an entrance connecting directly between Historic Columbia River Highway and the building interior. The entrance must be open to the public during all business hours.*
- B. *Building entrances must be architecturally emphasized and visible from the street.*
- C. *Due to the elevation difference between the Historic Columbia River Highway and the public parking lot, daylight basement type of construction is preferred. This method of construction has the benefit of causing all materials stored below Historic Columbia River Highway to be screened and secured behind sight-obscuring walls, adding to the attractiveness of the downtown area.*

Findings: The site does not have frontage on the Historic Columbia River Highway therefore subsections A and C do not apply. The building entrances have been architecturally emphasized, particularly for Building A, which has the highest visibility from the front lot line on SW Kendall Ct. The prominent plaza and community space provide the emphasis directing pedestrians to the public entrance. These standards are met.

A.108 Building Facades.

- A. *Ornamental devices, such as molding, entablature, and friezes, are encouraged at the roofline. Where such ornamentation is present in the form of a linear molding or board, the band must be at least eight (8) inches wide.*
- B. *Alcoves, Porches, Arcades, etc. Buildings must incorporate features such as arcades, roofs, porches, alcoves, porticoes, and awnings to protect pedestrians from the rain and sun. Awnings and entrances may be designed to be shared between two structures.*
- C. *Traditional Storefront Elements. For buildings designed to house retail, service, or office businesses, traditional storefront elements are required. These elements include:*
 - 1. *Front and side building walls placed within ten (10) feet of abutting street right-of-way boundaries.*
 - 2. *Clearly delineated upper and lower facades.*
 - 3. *A lower facade dominated by large display windows and a recessed entry or entries.*
 - 4. *Smaller, regularly spaced windows in the upper floor.*
 - 5. *Decorative trims, such as window hoods, surrounding upper floor windows.*
 - 6. *A decorative cornice near the top of the facade.*
- D. *Change in Relief of Building. Buildings must include changes in relief on ten (10%) percent of their Historic Columbia River Highway facades. Relief changes include cornices, bases, fenestration, fluted masonry, or other treatments for pedestrian interest and scale.*

Findings: The site does not have frontage on the Historic Columbia River Highway, therefore subsection D does not apply. Only residential uses are proposed so subsection C also does not apply. As shown on the Sheets A4.00—A4.05, the necessary ornamentation is shown at the roofline and the porches and outside seating areas feature covered areas. These standards are met.

A.109 Windows.

- A. *Windows which allow views to the interior activity or display areas are encouraged. Windows shall include sills at the bottom and pediments at the top. Glass curtain walls, reflective glass, and painted or darkly tinted glass shall not be used.*

Findings: The most prominent building element from the public sidewalk is the community room on SW Kendall Ct, which has a storefront window system that allows views into the interior activity. This standard is met.

- B. *Ground Floor Windows. All new buildings must provide ground floor windows along Historic Columbia River Highway.*
 - 1. *Required window areas must be either windows that allow views into working areas or lobbies, pedestrian entrances, or display windows.*
 - 2. *Required windows must have a sill no more than four (4) feet above grade. Where interior floor levels prohibit such placement, the sill must be raised to allow it to be no more than two (2) feet above the finished floor level, up to a maximum sill height of six (6) feet above grade.*
 - 3. *Glass curtain windows are not permitted fronting Historic Columbia River Highway.*
 - 4. *Darkly tinted windows and mirrored windows that block two-way visibility are prohibited as ground floor windows along street facades.*
 - 5. *Any wall that faces Historic Columbia River Highway must contain at least twenty percent (20%) of the ground floor wall area in display areas, windows, or doorways. Blank walls are prohibited.*

6. *Ground floor windows are also required on facades facing any public parking lot. The minimum requirement is sixteen (16) square feet per story or six percent (6%) of the facade, whichever is greater.*

Findings: The site does not have frontage on the Historic Columbia River Highway; therefore, this standard does not apply.

C. *Upper Floor Window Standards.*

1. *Glass area dimensions shall not exceed 5'x7'. (The longest dimension may be taken either horizontally or vertically.)*
2. *Windows must have trim or molding at least two (2) inches wide around their perimeters.*
3. *At least half of all the window area in upper floors must be made up of glass panes with dimensions no greater than 2'x3'. Windows that have 1'x1' grid inside double-pane glass are appropriate and are encouraged.*

Findings: As shown on Sheets A4.00—A4.03 and A6.00—A6.01, the upper floor windows of the residential units have been provided with large windows to provide more day light and views for residents. The proposed windows are 7' x 6'-6" and have a 1" protrusion for window trim. These are large panes and are not broken up into smaller segments. As these format windows will better serve the residential use and will create a better visual rhythm to the building facades, an adjustment to this standard is requested and discussed with the other adjustments to design standards in 8.240, above.

A.110 *Landscaping/Streetscape.*

- A. *Benches, outdoor seating, and trash receptacles must complement the existing ornamental street lighting and be in keeping with the overall architectural character of the downtown.*
- B. *Benches and other streetscape items may be placed within the public right-of-way, but must not block free movement of pedestrians. A minimum pedestrian walkway width of five (5) feet must be maintained at all times.*

Findings: As shown on Figures L1.00—L1.03, the project landscape architect has placed ornamental furniture that will complement the architectural character of downtown but has not placed it in the right-of-way. These standards are met.

A.111 *Lighting.*

- A. *All building entrances and exits must be well lighted.*
- B. *Exterior lighting must be an integral part of the architectural design, complement the existing ornamental street lighting, and be in keeping with the overall architectural character of the downtown.*
- C. *The minimum lighting level for building entries is four (4) foot-candles. Lighting must be a pedestrian scale and the source light must be shielded to reduce glare.*

Findings: As discussed in Chapter III, a lighting plan for the site has been developed that will provide shield lighting of entrances, pathways, and parking lots without intruding on surrounding properties.

A.112 *External Storage. The external storage of merchandise and/or materials directly or indirectly related to a business is prohibited.*

A.113 *Outdoor Seasonal Displays of Merchandise. Outdoor seasonal displays of merchandise are permitted during business hours only. A minimum pedestrian walkway width of five (5) feet must be maintained at all times.*

Findings: As a proposed residential-only use, there is not a business use that would entail exterior storage or display. These standards are met.

A.114 Trash and Recycling Storage.

- A. *Each structure shall provide for collection of its trash and recyclable materials within the boundaries of each parcel.*
- B. *All trash collection areas must be located within the structure, or behind the building in an enclosure, in accordance with the provisions of Chapter 11.015, Garbage and Recycling Container Enclosures, of this Code.*

Findings: As shown in Sheets A1.50, the trash and recycling enclosures on the site are located behind the buildings they serve and on the site. As discussed above, the standards of 11.015 are also met.

A.115 Signage.

Findings: Proposed signage for the site, including wayfinding will meet these standards at the time of development.

A.116 Additional Requirements. The provisions of this appendix to the Troutdale Development Code shall be applicable to any construction or change in use subject to site and design review. The Site and Design Review Committee will evaluate construction for compliance with these provisions and guidelines in the Downtown Concept Plan.

Findings: The proposed development is subject to site and design review, therefore, these provisions apply. The proposal is consistent with the 2020-2040 Town Center Plan as discussed above.

TMC Chapter 8.26 – Outdoor Lighting

8.26.050 - Submission of lighting plans and evidence of compliance for new development.

All proposed development, other than a detached single-family dwelling, that includes outdoor light fixtures shall submit a lighting plan in compliance with this chapter as part of any required application for land use approval. The plan shall include, but is not limited to, the following, all or part of which may be in addition to the information required with the application for the land use approval:

1. *Plans showing the location on the premises of all outdoor light fixtures and the type of light fixtures, lamps, supports, reflectors and other devices that may be part of or related to any part of an outdoor light fixture.*
 2. *Descriptions of the light fixtures, lamps, supports, reflectors and other devices. The description may include, but is not limited to, catalog cuts by manufacturers and drawings.*
 3. *The wattage for each light fixture.*
 4. *A photometric report containing a numerical grid of light levels, measured in tenths of foot-candles that the fixtures will produce on the ground. For developments subject to the maximum light levels of Section 8.26.045, the numerical grid shall include light level measurements at property lines abutting residentially zoned property.*
 5. *For light fixtures requiring direct shielding, photometric data, or similar data showing the angle of cut off or line of sight of the fixture's lamp in relation to the property's boundaries.*
- B. *Additional Submission. The above required plans, descriptions and data shall be sufficiently complete to enable the director to readily determine compliance with the requirements of this chapter. If the director is unable to determine compliance with the requirements of this chapter from the plans, descriptions and data initially submitted by an applicant as required by this*

section, the director may require the applicant to submit additional information as is necessary to make such a determination. Additional information may include certified reports of tests performed by a recognized testing laboratory.

Findings: As discussed in Chapter III, a comprehensive light plan has been developed for the site that meets the intent of these regulations. A light plan demonstrating adherence to the specifics of this section will be submitted during the review. These standards will be met.

MEMORANDUM

DATE: June 8, 2022

TO: City of Troutdale

FROM: Dana Beckwith, PE, PTOE
Richard Martin, EIT

SUBJECT: Home Forward SW 157th Drive Trip Generation & Distribution Evaluation P21-068

This memorandum summarizes trip generation and distribution evaluation associated with the proposed Home Forward development at Multnomah County Tax Lots 1N3E25CB -02502, 1N3E25CB -02501, and 1N3E25CB -02601 in Troutdale, Oregon. The development proposes 94 affordable multifamily housing units and has site frontages on SW 157th Drive, SW Kendall Court, SW 2nd Street, and SW 4th Street. This trip generation and distribution evaluation considers the following elements:

- Introduction
- Existing Conditions
 - Sight Distance Evaluation
 - Existing Traffic Volumes
- Future Conditions
 - Background Growth
 - Trip Generation and Trip Distribution
 - 2024 Background Year Analysis
 - 2024 Buildout Year Analysis
- Appendix

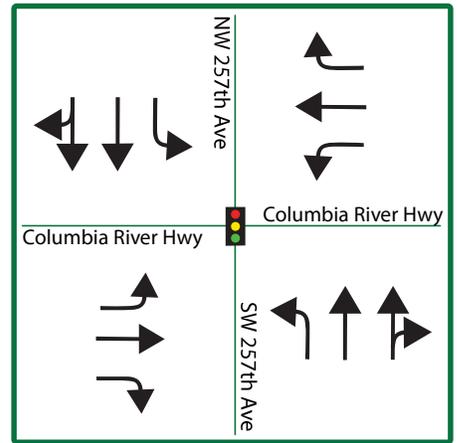
BACKGROUND

The multifamily development will be broken down as follows:

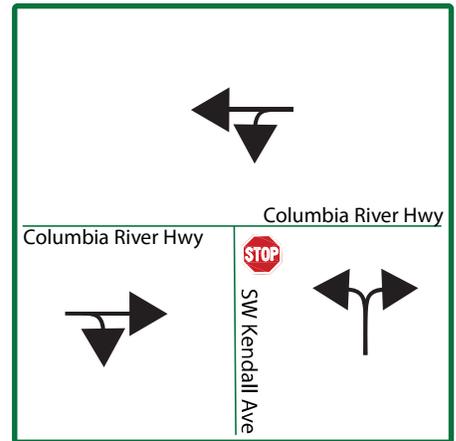
- 18 studio units
- 29 one-bedroom units
- 35 two-bedrooms units
- 12 three-bedroom units.

This breakdown is based on a 94-unit count. Figures 1 and 2 shows the vicinity map and preliminary site plan. Traffic count data was collected at the following intersections to determine trip distribution for the proposed development:

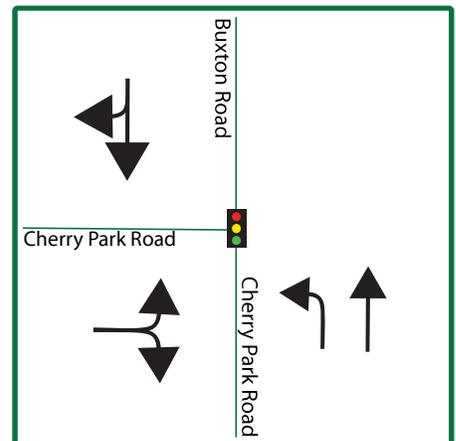
- SW 257th Drive at Columbia River Highway
- SW Kendall Avenue at Historic Columbia River Highway
- Buxton Road / S Troutdale Road at SW Cherry Park Road



① SW 257th Avenue at Columbia River Highway



② SW Kendall Avenue at Columbia River Highway



③ Buxton Road at SW Cherry Park Road



- Project Site
- = Traffic Signal
- = Study Intersection
- = Stop Sign
- = Existing Lane Configuration

Figure 1: Vicinity Map

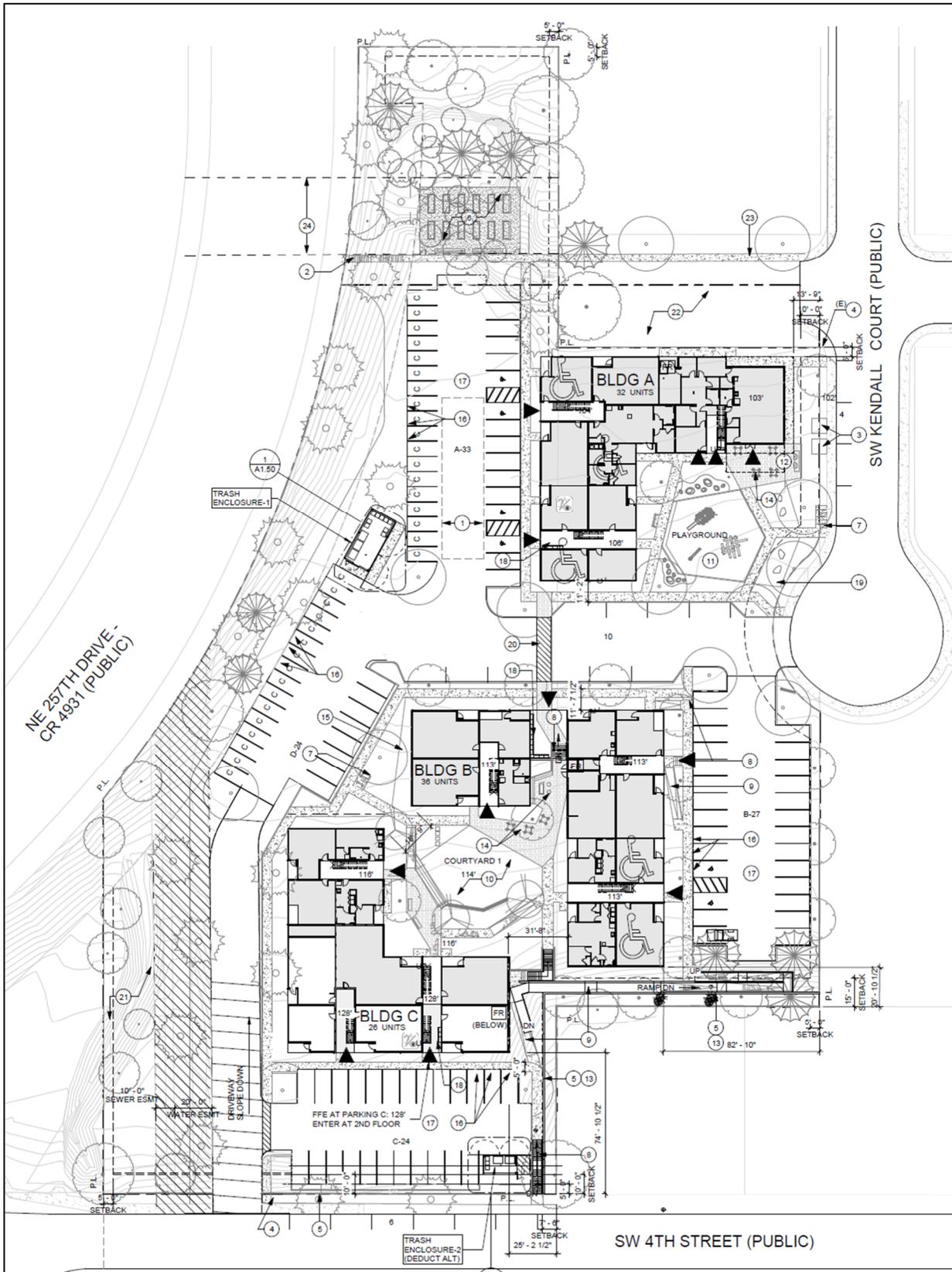


Figure 2: Preliminary Site Plan

EXISTING CONDITIONS

Existing transportation conditions were evaluated at the study intersections. All modes of travel were evaluated, including pedestrians, bicycles, transit and motor vehicles. The existing transportation conditions are summarized in Table 1.

Table 1: Roadway Characteristics

Roadway	Average Daily Traffic ¹	Functional Classification ²	Posted Speed Limit	Sidewalks	Transit	Bike Lanes	Lane Geometry	On-Street Parking
Columbia River Highway	9,100	Collector	20 mph	Both sides	Trimet Bus Line #80	Both sides east of Kendall Ave	One 11'-12' lanes in each direction	Both sides between Buxton Road and Kibling Street
SW 257th Drive	20,500	Arterial	35 mph	Both sides	Trimet Bus Line #81	Both sides	Two 11'-12' lanes in each direction and a center median.	None
SW Kendall Avenue	260	Local Street	25 mph	Both sides	None	None	One ~30' cross-section to accommodate two-way travel and on-street parking.	Both sides
SW 4th Street	-	Local Street	25 mph	North side, intermittent	None	None	One ~28' cross-section to accommodate two-way travel and on-street parking.	Both sides
Buxton Road	6,770	Collector	25 mph	Both sides	Trimet Bus Line #80	None	One 17'-18' lane in each direction, on-street parking included.	Both sides
SW Cherry Park Road	1,640	Collector	30 mph	Both sides	None	Both sides	One 11'-12' lane in each direction	None

Notes: ¹Based on the PM peak hour count data collected in 2022.
²Based on the City of Troutdale TSP

SIGHT DISTANCE EVALUATION

Intersection and stopping sight distances were evaluated for the proposed site accesses on SW Kendall Avenue and SW 4th Street. The sight distance evaluation follows the guidance provided in the American Association of State Highway and Transportation Official's (AASHTO) Geometric Design of Highways and Streets, 2011. For a 25-mph roadway, AASHTO requires 155 feet for stopping sight distance and 280 feet for intersection sight distance. AASHTO also requires an assumed object height of 4.5 feet, a driver's eye height of 3.5 feet, and a driver's setback of 14.5 feet from the traveled way. Intersection sight distance was compared to the AASHTO design intersection sight distance for the following cases:

- Case B1, Left Turn from the Minor Road¹

¹ AASHTO, Case B1 – Intersections with stop control on the minor road (AASHTO, Case B1, Table 9-6).

- Case B2, Right Turn from the Minor Road²

Table 2 shows a summary of the sight distance evaluation. Sight distance for the SW Kendall Avenue accesses was found to be clear to the north all the way to the intersection with Historic Columbia River Highway, which was approximately 260 feet from the northern site access and 415 feet from the southern site access. Sight distance to the north was found to be clear to the end of the cul-de-sac at the Multnomah County Sheriff's Office, which was approximately 200 feet from the northern site access (the southern site access will share the cul-de-sac for access).

The distance from the proposed northern access on SW Kendall Avenue to the neighboring intersections is less than the minimum required sight distance, which means that the turning speed for vehicles on Historic Columbia River Highway was used for the stopping sight distance in the analysis. Even though it is not possible to meet the minimum intersection sight distance due to the short distance between intersections, safe operation at the intersection is possible because stopping sight distance is met.

Sight distance for the SW 4th Street access was found to be clear in both directions as well. To the east, sight distance is clear for approximately 600 feet to the intersection at Buxton Road. To the west, sight distance is clear for approximately 100 feet to the end of the public roadway at the Troutdale Public Works entrance. Vehicles exiting Troutdale Public Works will be driving at low enough speeds that safe operation of the driveway should be possible. No sight distance mitigations are recommended.

Table 2: Sight Distance Evaluation

Sight Distance Evaluated	Estimated Available Sightline (ft)	Sight Distance Standards (ft)	Meets Standard?
Proposed northern Kendall Ave Site Access			
Case B1: Left-turn	To the north ≈ 260	280	No
	To the south ≈ 200	280	No
Case B2: Right-turn	≈ 260	240	Yes
SSD – SB on SW Kendall Ave	≈ 200	95	Yes
SSD – NB on SW Kendall Ave	≈ 260	155	Yes
Proposed southern Kendall Ave Site Access			
Case B1: Left-turn	To the north ≈ 415	280	Yes
	To the south ≈ N/A	280	N/A
Case B2: Right-turn	≈ 415	240	Yes
SSD – SB on SW Kendall Ave	≈ N/A	155	N/A
SSD – NB on SW Kendall Ave	≈ 415	155	Yes
Proposed SW 4th Street Site Access			
Case B1: Left-turn	To the east ≈ 600	280	Yes
	To the west ≈ N/A	280	N/A
Case B2: Right-turn	≈ 600	240	Yes
SSD – EB on SW 4 th Street	≈ N/A	155	N/A
SSD – WB on SW 4 th Street	≈ 600	155	Yes

² AASHTO, Case B2 – Intersections with stop control on the minor road (AASHTO, Case B2, Table 9-8).

TRAFFIC COUNT DATA

Existing traffic count data was collected at the intersections during the PM peak period of 4:00 to 6:00 PM on February 24, 2022. The peak hour turning movements are shown in Figure 3 and detailed traffic count data is provided in the Appendix.

FUTURE CONDITIONS

Background growth and site-generated trips were added to existing volumes to develop the following scenarios:

- 2024 Background – existing peak hour volumes plus background growth over two years
- 2024 Buildout – 2024 Background volumes plus site-generated trips.

Background Growth

A background growth rate of two percent per year was used to develop the 2024 background conditions volumes.

2024 Background Traffic Volumes

The background growth rates were applied to the existing peak hour volumes to develop the 2024 background traffic volumes. This represents the projected traffic volumes along the transportation network without the proposed development. The 2024 Background traffic volumes are shown in Figure 4.

2024 Trip Generation and Trip Distribution

Trip generation rates were taken from the Institute of Transportation Engineer’s (ITE) *Trip Generation Manual, 11th Edition* based on the proposed land use. Rated average or fitted curve equations as recommended by Chapter 4 of the ITE *Trip Generation Handbook* will be utilized in the trip generation development. Estimates were developed for AM, PM, and Average Daily Trips (ADT).

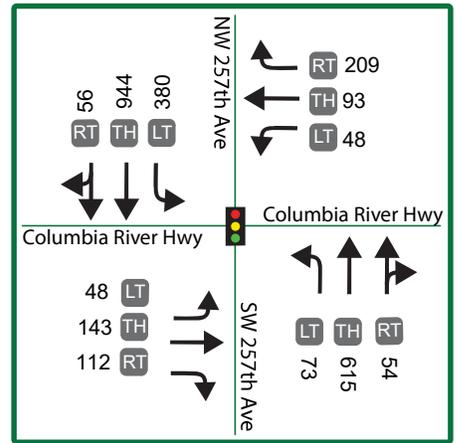
The proposed development will provide 94 affordable multi-family housing units at the site. In review of ITE land use codes, the trips generated by the development will most closely be represented by the ITE Land Use Code 223 – Affordable Housing, based on the number of units. Table 3 summarizes an estimate of the trip generation.

Table 3: Trip Generation Summary

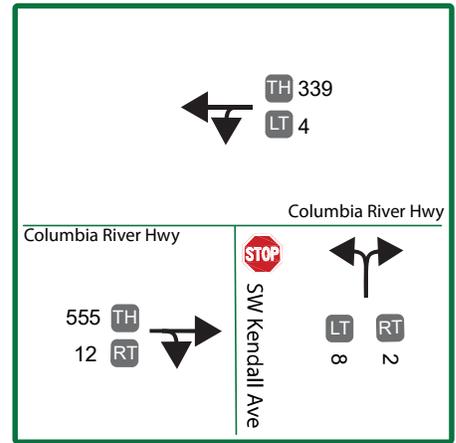
ITE Land Use Code	Size	Units	Rate Used			AM Peak Hour			PM Peak Hour			Weekday Daily Total
			AM ³	PM	Daily	Total	In	Out	Total	In	Out	
223 – Affordable Housing	94	Units	0.39	0.46	4.81	37	11	26	43	25	18	452

The peak hour turning movement counts obtained at study intersections were used to distribute site generated vehicle traffic. Trip distribution is shown in Figure 5.

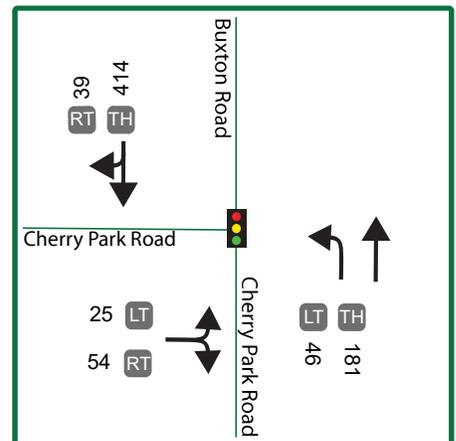
³ Rate based on the fitted curve equation $T = 0.21(X) + 17.21$ that best represents affordable housing land use according to ITE.



① SW 257th Avenue at Columbia River Highway



② SW Kendall Avenue at Columbia River Highway

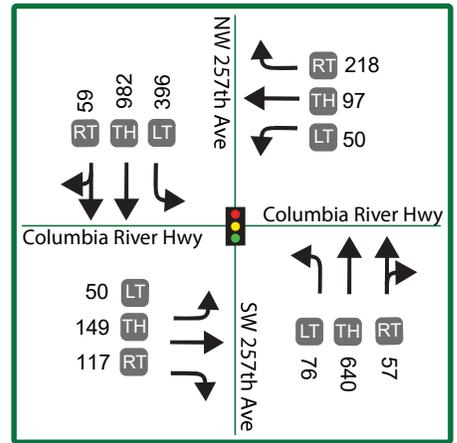


③ Buxton Road at SW Cherry Park Road

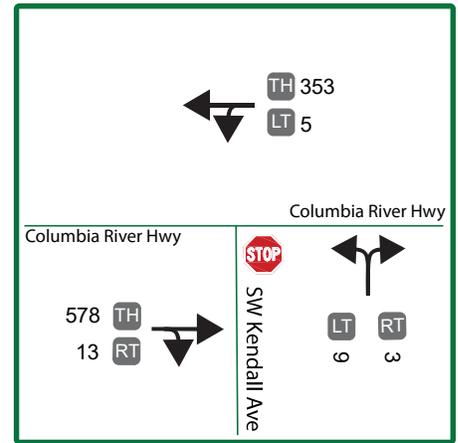


- Project Site
- = Traffic Signal
- = Study Intersection
- = Stop Sign
- = Existing Lane Configuration
- = Left / Through / Right Turn

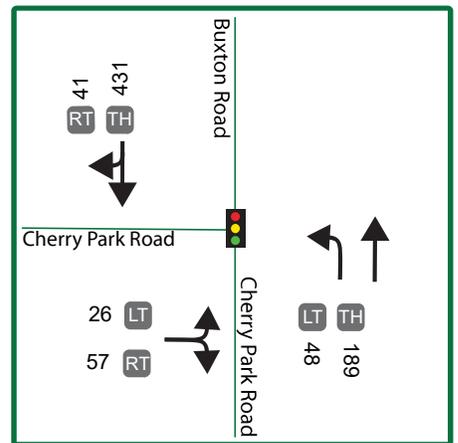
Figure 3: Traffic Volumes 2022 Existing Conditions PM Peak Hour



① SW 257th Avenue at Columbia River Highway



② SW Kendall Avenue at Columbia River Highway

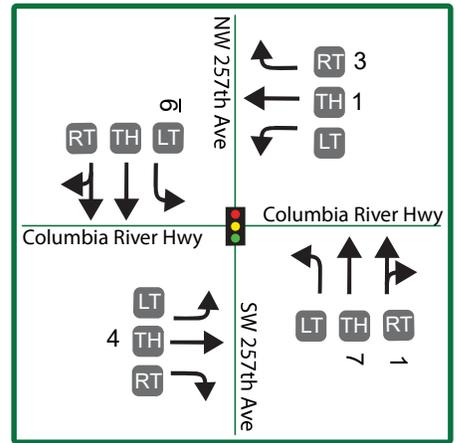


③ Buxton Road at SW Cherry Park Road

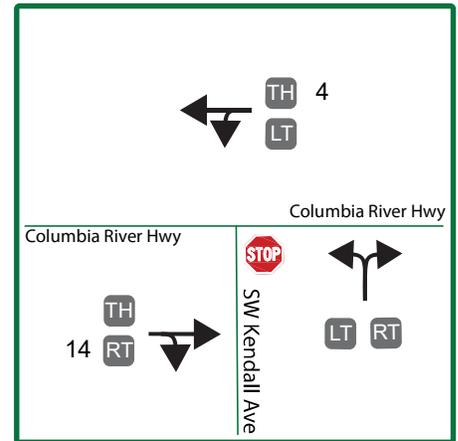


- Project Site
- = Traffic Signal
- = Study Intersection
- = Stop Sign
- = Existing Lane Configuration
- = Left / Through / Right Turn

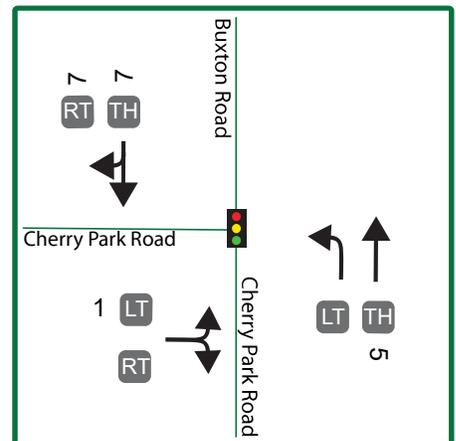
Figure 4: Traffic Volumes 2024 Background Conditions PM Peak Hour



① SW 257th Avenue at Columbia River Highway



② SW Kendall Avenue at Columbia River Highway



③ Buxton Road at SW Cherry Park Road



- Project Site
- = Traffic Signal
- = Study Intersection
- = Stop Sign
- = Existing Lane Configuration
- = Left / Through / Right Turn

Figure 5: Traffic Volumes Trip Distribution PM Peak Hour

2024 Buildout Traffic Volumes

To develop the 2024 Buildout traffic volumes, project site generated trips were distributed for the PM peak hour and added to the 2024 background volumes. The 2024 Buildout traffic volumes are presented in Figure 6.

Alternative Land Use Comparison

Other land uses were investigated to determine possible maximum traffic impacts for developments allowed outright under existing zoning⁶. Table 4 shows two trip generation scenarios that are possible under existing zoning, based on the ITE land use codes 822 – Strip Retail Plaza (<40k) and 881 – Pharmacy/Drug Store with Drive Through Window.

Table 4: Alternative Trip Generation Summary

ITE Land Use Code	Size	Units	Average Rate			AM Peak Hour			PM Peak Hour			Weekday Daily Total
			AM	PM	Daily	Total	In	Out	Total	In	Out	
Scenario 1: 822 – Strip Retail Plaza	15	Ksf	2.36	6.59	54.45	35	21	14	99	49	50	817
Scenario 2: 881 – Pharmacy / Drug Store with Drive Through Window	15	Ksf	3.74	10.25	108.4	56	29	27	154	77	77	1626

As shown in Table 4, other possible uses for the site could generate significantly more trips than the proposed affordable housing development.

Local Traffic Concerns

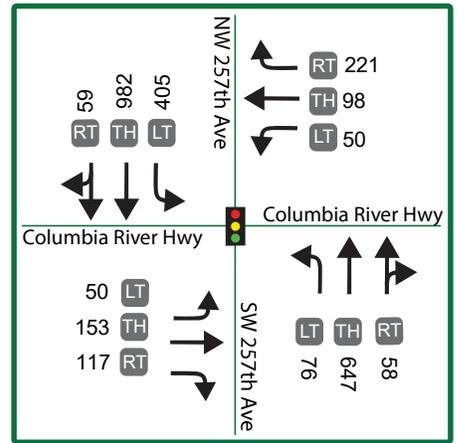
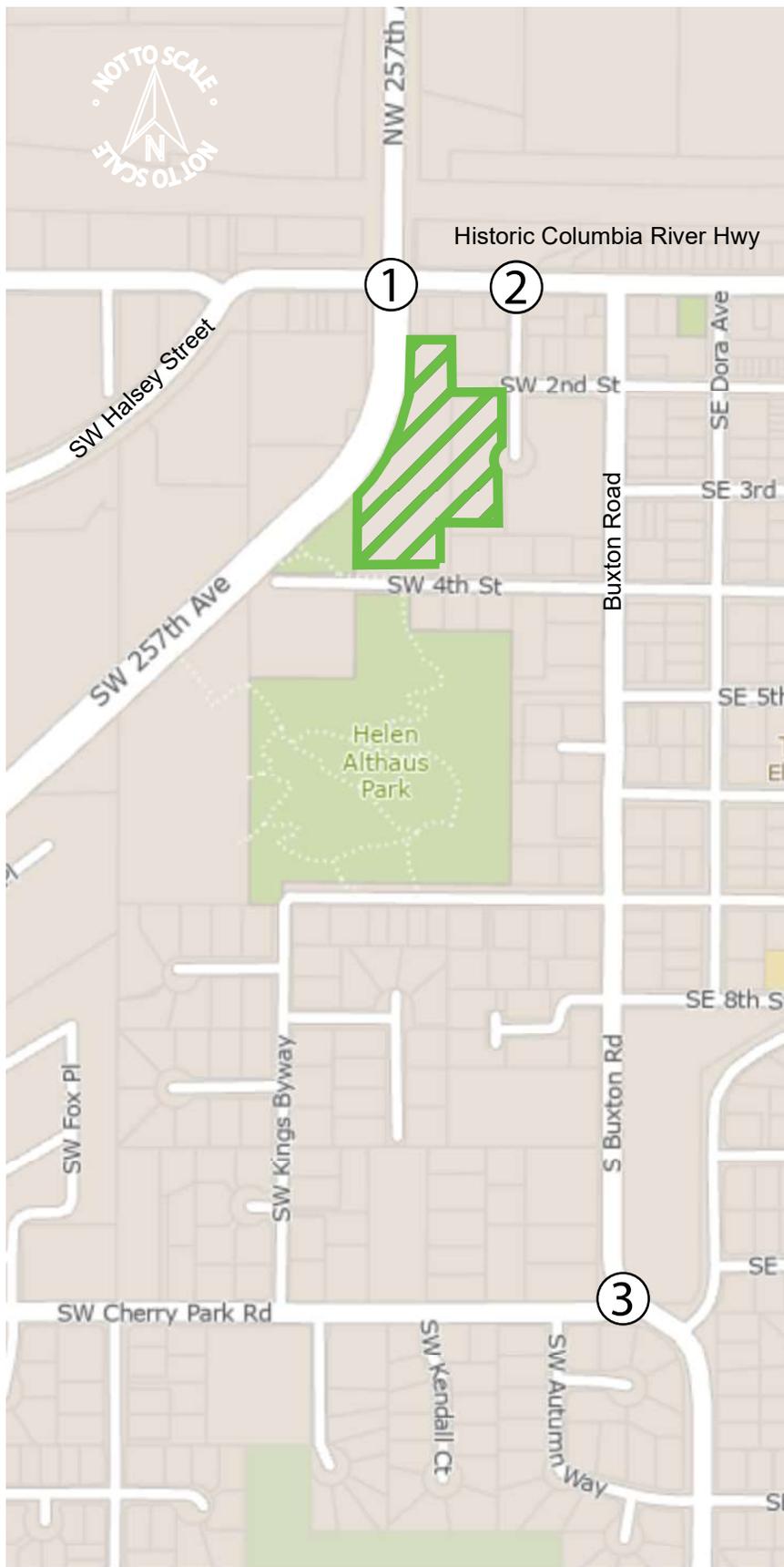
Pass through trips was identified as a concern along SW 4th Street west of Buxton Road with the addition of the development. To address this potential, a dead-end sign can be located at the eastern end of SW 4th Street where it intersects with Buxton Road. This method would deter vehicles on Buxton Road unrelated to the development from creating additional traffic on SW 4th Street.

CONCLUSION

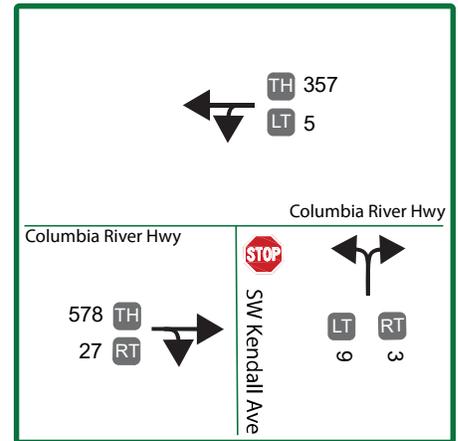
A 94-unit affordable multifamily housing development is proposed for Multnomah County Tax Lots 1N3E25CB -02502, 1N3E25CB -02501, and 1N3E25CB -02601 in Troutdale, Oregon. The development will access SW Kendall Avenue and SW 4th Street.

Traffic count data was collected at three study intersections during the PM peak period of 4:00 PM to 6:00 PM. Based on the traffic count data, inbound site trips are expected to access the site via Historic Columbia River Highway to SW Kendall Avenue and Buxton Road to SW 4th Street or SW 2nd Street. Outbound site trips are expected to access Buxton Road via SW 2nd Street or SW 4th Street, then will travel northbound to Historic Columbia River Highway or southbound to SW Cherry Park Road.

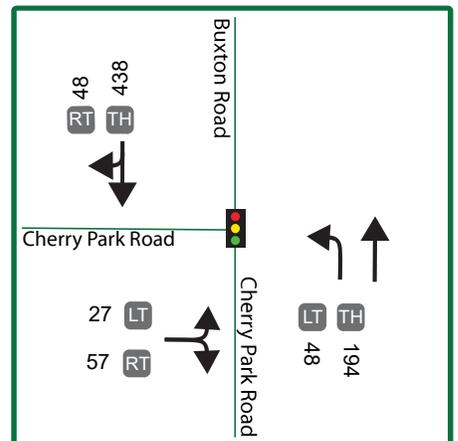
⁶ Troutdale Development Code Section 3.123.



① SW 257th Avenue at Columbia River Highway



② SW Kendall Avenue at Columbia River Highway



③ Buxton Road at SW Cherry Park Road



- Project Site
- = Traffic Signal
- = Study Intersection
- = Stop Sign
- = Existing Lane Configuration
- = Left / Through / Right Turn

Figure 6: Traffic Volumes 2024 Buildout Conditions PM Peak Hour

Trip generation and distribution was developed for the proposed development based on land use codes from the ITE Trip Generation Manual. Trip generation was also developed for two other possible land use codes that could be allowed on the site based on the existing zoning and development codes. According to the analysis, an affordable housing development would generate an estimated 37 AM and 43 PM peak hour trips which is significantly fewer new trips than other land uses allowed under existing zoning. These new trips are likely to have minimal impacts on the roadway network.

One method to address concerns raised for the potential of site pass through trips is to add a dead-end sign to the eastern end of SW 4th Street where it intersects with Buxton Road. This method would deter vehicles on Buxton Road unrelated to the development from creating additional traffic on SW 4th Street and is supported by the development.

MEMORANDUM

DATE: April 12, 2022
TO: City of Troutdale
FROM: Dana Beckwith, PE, PTOE
 Richard Martin, EIT

SUBJECT: Home Forward SW 157th Drive Parking Study

P21-068

This memorandum summarizes the parking study associated with the proposed Home Forward development at Multnomah County Tax Lots 1N3E25CB -02502, 1N3E25CB -02501, and 1N3E25CB -02601 in Troutdale, Oregon. The development proposes 94 affordable multifamily housing units and has site frontages on SW 157th Drive, SW Kendall Court, SW 2nd Avenue, and SW 4th Avenue.

BACKGROUND

The multifamily development will be broken down as follows:

- 18 studio units
- 29 one-bedroom units
- 35 two-bedrooms units
- 12 three-bedroom units.

This breakdown is based on a 94-unit count. Figures 1 and 2 shows the vicinity map and preliminary site plan.

Based on Troutdale Development Code Chapter 9: *Off-Street Parking and Loading*, the required number of parking spaces for the development based on the proposed use is 188 for the 94-unit development, as summarized in Table 1. The development proposes 119 off-street parking spaces and 11 on-street parking spaces.

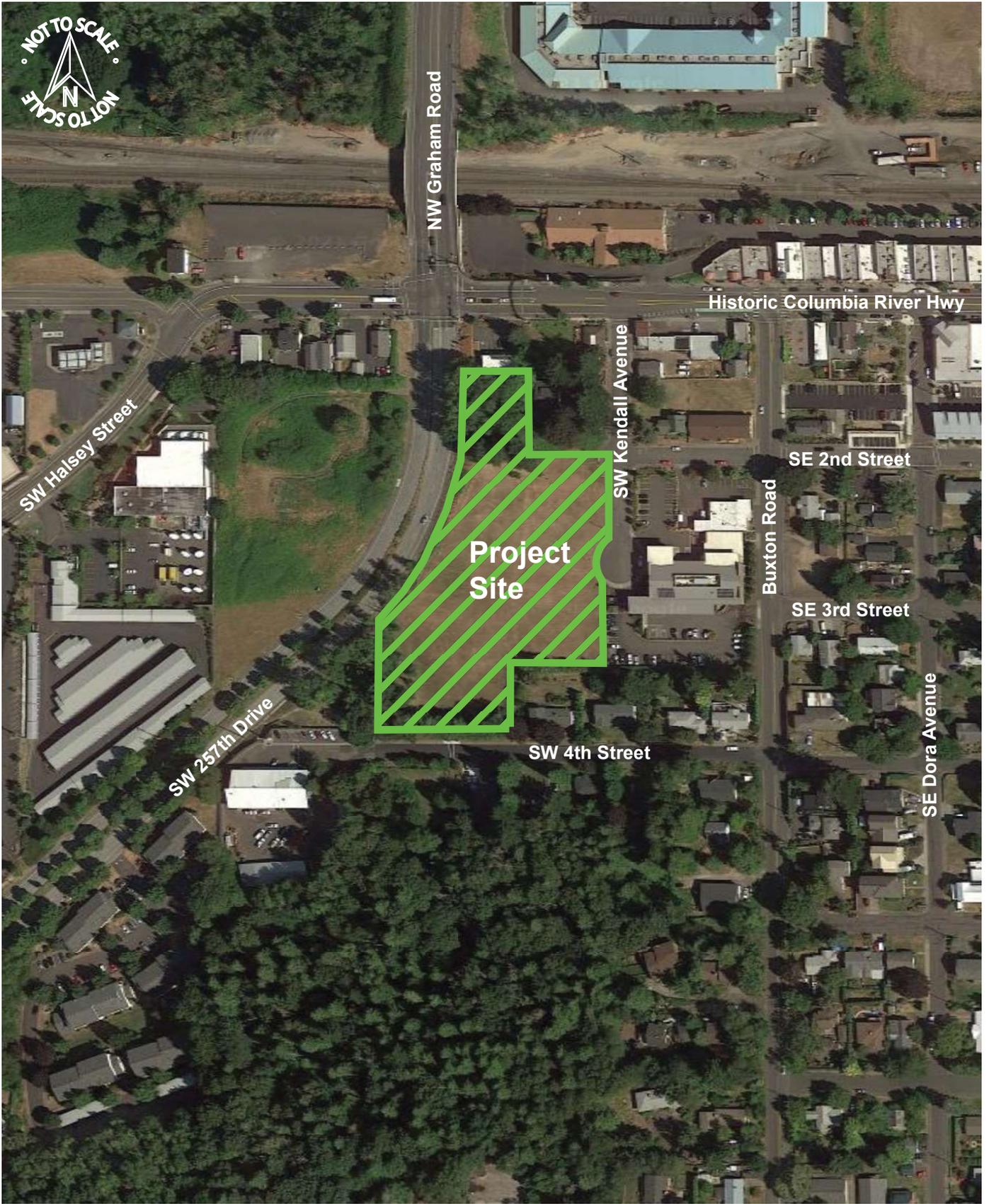
Table 1: City Parking Requirements and Proposed Spaces

Type of Use	Comments	Required Spaces per Unit	Proposed Units	Total Spaces Required	Proposed Parking Spaces
Multiple-family dwelling (apartments)	2 spaces per dwelling unit	2.0	94	188	130

TROUTDALE PARKING REDUCTION POLICIES

Troutdale Development Code Section 9.040 outlines possible reductions for the required number of parking spaces. Reductions are allowed for developments within 400 feet of transit routes, developments that provide transit amenities, and developments with adjacent on-street parking available.

The transit reduction allows for a 10% reduction by providing a transit stop and related amenities at the site. The proposed development is within 400 feet of multiple existing transit routes to the north. Based on Troutdale Development Code Section 9.040(B), this development does not qualify for this reduction.



Project Site

Figure 1: Vicinity Map



Figure 2: Preliminary Site Plan

The on-street parking reduction allows for a reduction of one off-street space for every possible on-street space on street frontages directly adjacent to the development. Based on the available site curbside parking along the site frontage, the development shows six on-street parking spaces on SW 4th Street and five spaces on SW Kendall Court.

Seeking the reduction for the 11 on-street parking spaces as outlined in the City's Development Code will result in a required 177 off-street spaces.

PARKING REDUCTION RESEARCH

Affordable housing developments are not listed specifically in either the parking requirements section or the parking reductions section of the City's Development Code. A comparison to other Troutdale vicinity multi-family and affordable housing sites was conducted to determine the parking utilization for similar developments.

Parking Demand Observations

Overnight parking demand data was collected at five affordable housing sites in Troutdale and Fairview. The data collected is shown in Table 2. Analysis based on the data is shown in Table 3. The detailed data is provided in Appendix A.

Table 2: Parking Data Summary

Apartment Complex	Inventory			Number of Cars Parked Between 11PM-12AM			Number of Cars Parked Between 12AM-1AM		
	Standard Stalls	ADA Stalls	Loading Zones	Standard Stalls	ADA Stalls	Loading Zones	Standard Stalls	ADA Stalls	Loading Zones
Troutdale Terrace	395	6	0	338	2	0	339	2	0
Cherry Ridge	254	11	0	142	5	0	146	5	0
Fairview Arms	61	7	0	40	2	0	41	2	0
Fairview Oaks & Woods	346	15	0	259	5	0	270	8	0
Kings Garden	241	7	0	187	4	0	189	4	0
Average	259	9	0	193	4	0	197	4	0

Table 3: Parking Demand Analysis Summary

Apartment Complex	On-Site Parking Spaces	Average Parking Demand	Apartment Units	Occupied Stalls Per Unit
Troutdale Terrace	401	341	228	1.49
Cherry Ridge	265	149	160	0.93
Fairview Arms	68	43	45	0.94
Fairview Oaks Woods	361	271	328	0.83
Kings Garden	248	192	152	1.26
Average				1.09

Table 3 shows that an average overnight parking demand of 1.09 stalls per unit can be expected of affordable housing developments in Troutdale and Fairview. Applying this rate to the proposed 94 units gives an anticipated overnight parking demand for this development of 103 spaces.

A field visit was conducted at these developments to photograph parking conditions in the evening on February 22nd, 2022. These photographs are included in Appendix B.

City of Beaverton

A 2020 parking study completed by Global Transportation Engineering in the City of Beaverton referenced data for two affordable housing projects:

- Spencer House Apartments at 13665 SW Larch Place
- Bridge Meadows Apartments at 5995 SW Menlo Drive

The data from these developments showed an average parking ratio of 0.74 vehicles per unit for the two developments. These two developments are served by just one bus line in their immediate vicinity, similar to the proposed Home Forward development.

Applying a rate of 0.74 spaces per unit to the proposed 157th Drive Development would result in an average demand of 70 spaces for the 94-unit proposal. Detailed parking data and vicinity maps that show site locations and transit availability are available in Appendix C.

Additional Affordable Housing Developments

Overnight parking data was collected in June 2020 at two additional affordable housing developments:

- Rockwood Station at 19100 East Burnside Street in Gresham, Oregon.
- Fairview Oaks & Woods at 22701 NE Halsey Street in Fairview, Oregon.¹

At the time of the parking data collection, Rockwood Station had 69 one-bedroom units and 107 two-bedroom units, for a total of 176 occupied units. Fairview Oaks & Woods had 44 one-bedroom units, 172 two-bedroom units, 83 three-bedroom units, and 10 four-bedroom units, for a total of 309 occupied units.

Rockwood Station showed 168 occupied parking spaces during data collection, which provides a parking rate of 0.95 occupied spaces per unit for the 176 occupied units. Fairview Oaks & Woods showed 356 occupied parking spaces during data collection, which provides a parking rate of 1.15 occupied spaces per unit for the 309 occupied units.

Based on an average rate of 1.05 spaces per unit, parking demand for the SW 157th Drive Development would result in 99 spaces for the 94-unit proposal. Detailed parking data for these sites are provided in Appendix D.

Parking Generation Manual

The Institute of Transportation Engineers, *Parking Generation Manual, 5th Edition* provides parking generation rates for affordable housing developments under Land Use Code 223 – Affordable Housing – Income Limits. Parking generation estimates for the proposed development were developed and are provided in Table 4.

¹ Fairview Oaks & Woods was re-counted for this study; the 2020 data provided in this section is provided as historic data for additional analysis.

Table 4: Parking Generation Requirements

Land Use	Code	Size (Dwelling Units)	Rate		Required Spaces	
			50 th Percentile	85 th Percentile	50 th Percentile	85 th Percentile
Affordable Housing – Income Limits	223	94	0.99	1.33	94	126

The demand numbers shown in Table 2 represent overnight parking demand, which is the highest parking demand period for a residential development. The ITE Parking Generation Manual draws from multiple similar sites for each land use in development of its rates. Table 2 shows an average parking demand of 94 spaces for 94 units. Applying the reduction for on-street parking of 11 spaces will result in 119 off-street parking spaces. This equates to a 37% reduction from the 188 spaces required by the City of Troutdale’s Development Code.

CONCLUSION

The City of Troutdale Development Code requires 188 spaces for the Home Forward development based on a development size of 94 multi-family units. However, the parking development code does not account for affordable housing in their parking code. This development proposes 119 off-street parking spaces and 11 on-street parking spaces for an affordable housing development and requests a parking requirements reduction.

Count data gathered at similar sites in Troutdale and Fairview indicated that parking rates for affordable housing developments can range from 0.83 to 1.49 spaces per unit, with an average demand of 1.09 spaces per unit. Assuming no other reductions, applying the average demand rate to the proposed 94-unit development yields an anticipated overnight peak parking demand of 103 spaces.

Overnight parking data collection at two similar sites in Beaverton showed parking demand rates of 0.71 and 0.77 spaces per unit. Based on an average overnight parking demand of 0.74 spaces per unit, this development shows overnight parking demand of 70 spaces per unit for 94 units.

Overnight parking demand data collected at two affordable housing developments in Gresham and Fairview showed parking demand rates between 0.95 and 1.15 spaces per unit. An average rate of 1.05 spaces per unit shows parking demand for the SW 157th Drive development of 99 spaces for 94 units.

Additional parking generation research using the *ITE Parking Generation Manual, 5th Edition* showed an average parking demand of 0.99 spaces per unit, which equates to a demand of 94 spaces for the 94-unit development.

Based on the research and reductions allowed by the City of Troutdale Development Code, it is anticipated that the proposed 119 off-street and 11 on-street parking spaces will be more than adequate to meet parking demand. Providing this level of parking is representative of a 1.38 parking spaces per housing unit ratio which exceeds the anticipated overnight parking demand identified in the summarized research. This parking ratio is recommended for approval.

Appendix

Appendix A: Parking Count Data



156803 Fairview and Troutdale Apartments

Collection Date: 1/19/2022-

Apartment Complex	Inventory			Parked cars between 11P-12A			Parked cars between 12A-11A			Demand Per Unit	
	Regular Stalls	ADA Stalls	Loading Zones	Regular Stalls	ADA Stalls	Loading Zones	Regular Stalls	ADA Stalls	Loading Zones	Total Units	Occupied Stalls Per Unit
Troutdale Terrace	395	6	0	338	2	0	339	2	0	228	1.49
Cherry Ridge	254	11	0	142	5	0	146	5	0	160	0.93
Fairview Arms	61	7	0	40	2	0	41	2	0	45	0.94
Fairview Oaks Woods	346	15	0	259	5	0	270	8	0	328	0.83
Kings Garden	241	7	0	187	4	0	189	4	0	152	1.26
Average	259	9	0	193	4	0	197	4	0	183	1.09



01 LANDSCAPE PLAN
1" = 80'-0"



PRECEDENT IMAGES



① COVERED PATIO WITH TABLES AT COMMUNITY ROOM



② DOUBLE BARBECUE



③ ORGANIC SEATING IN LAWN



④ TABLE SEATING AT PATIO



⑤ CONCRETE SEAWALL AT LAWN EDGE



⑥ ANGULAR PATIOS IN COURTYARDS



⑦ NATURE INSPIRED PLAY



⑧ BOULDER GARDEN AT PLAYGROUND



⑨ COMMUNITY GARDEN/EDIBLES



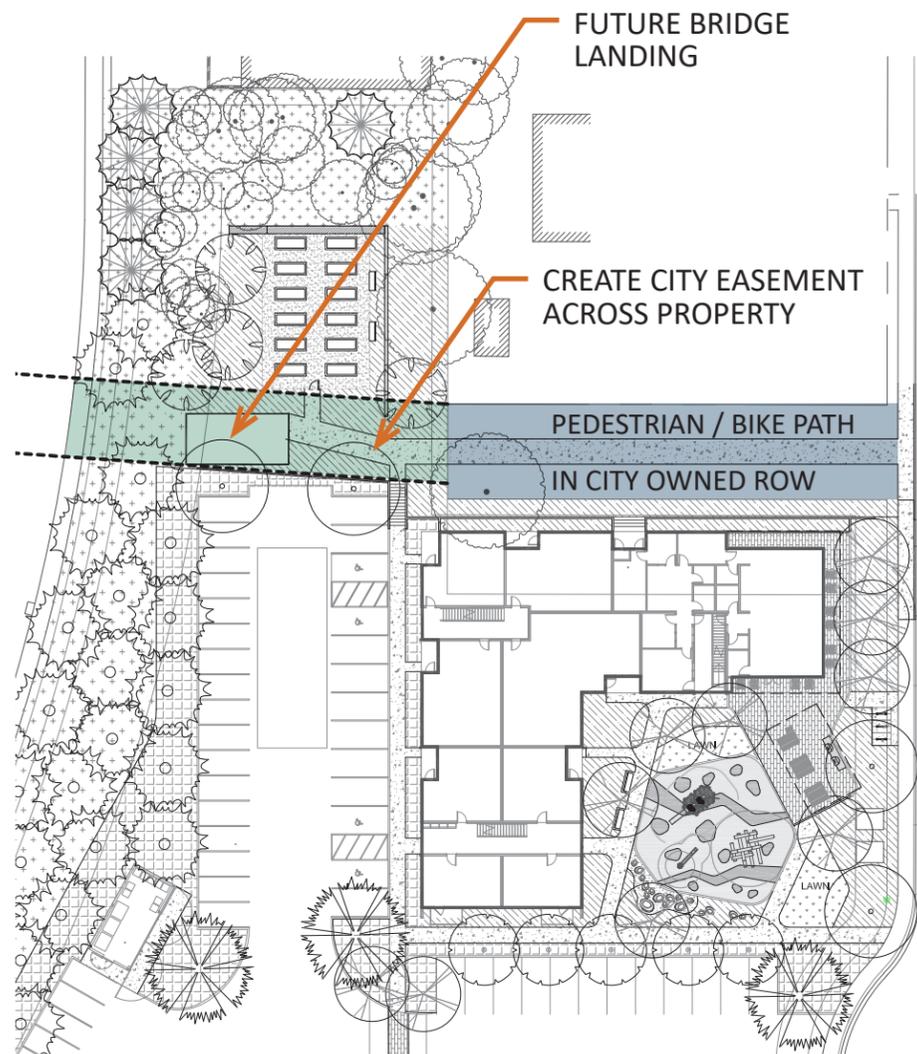
⑩ LUSH PLANTINGS IN COURTYARDS



⑪ OAK SAVANNAH ON HILLSIDE

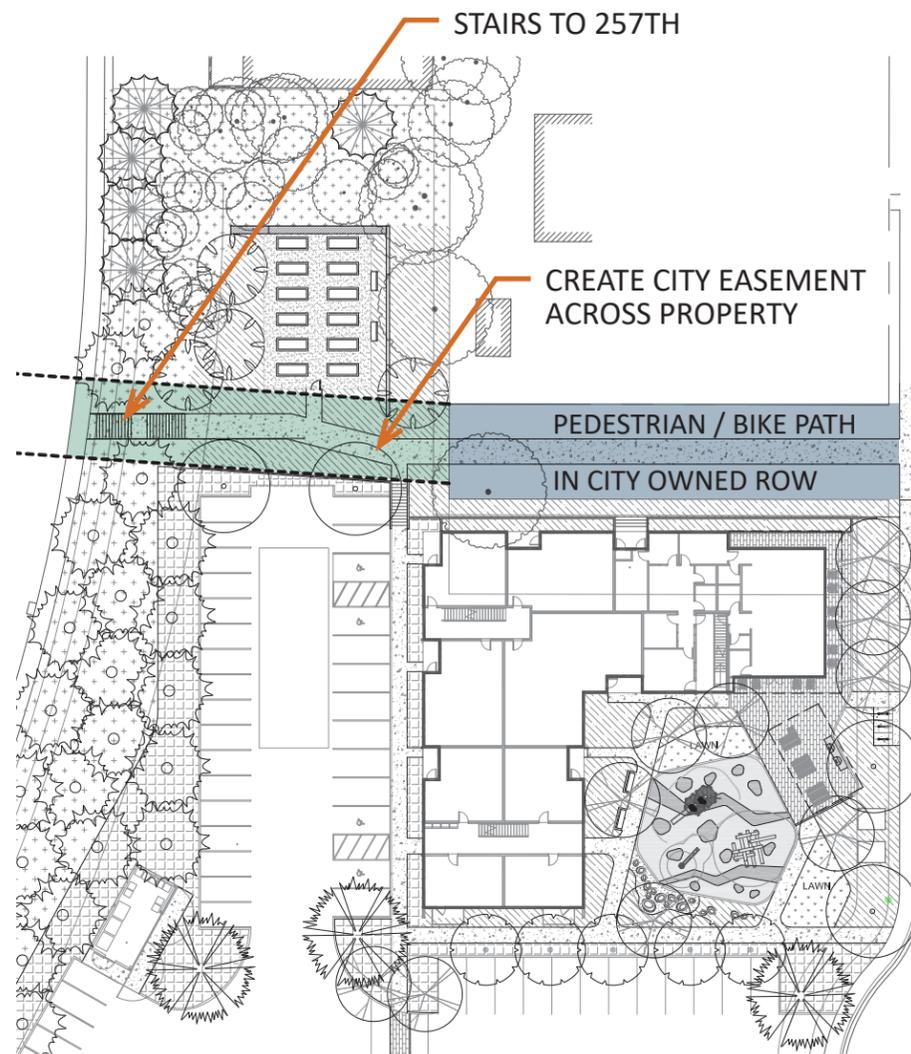


⑫ DENSELY PLANTED TOP OF HILLSIDE



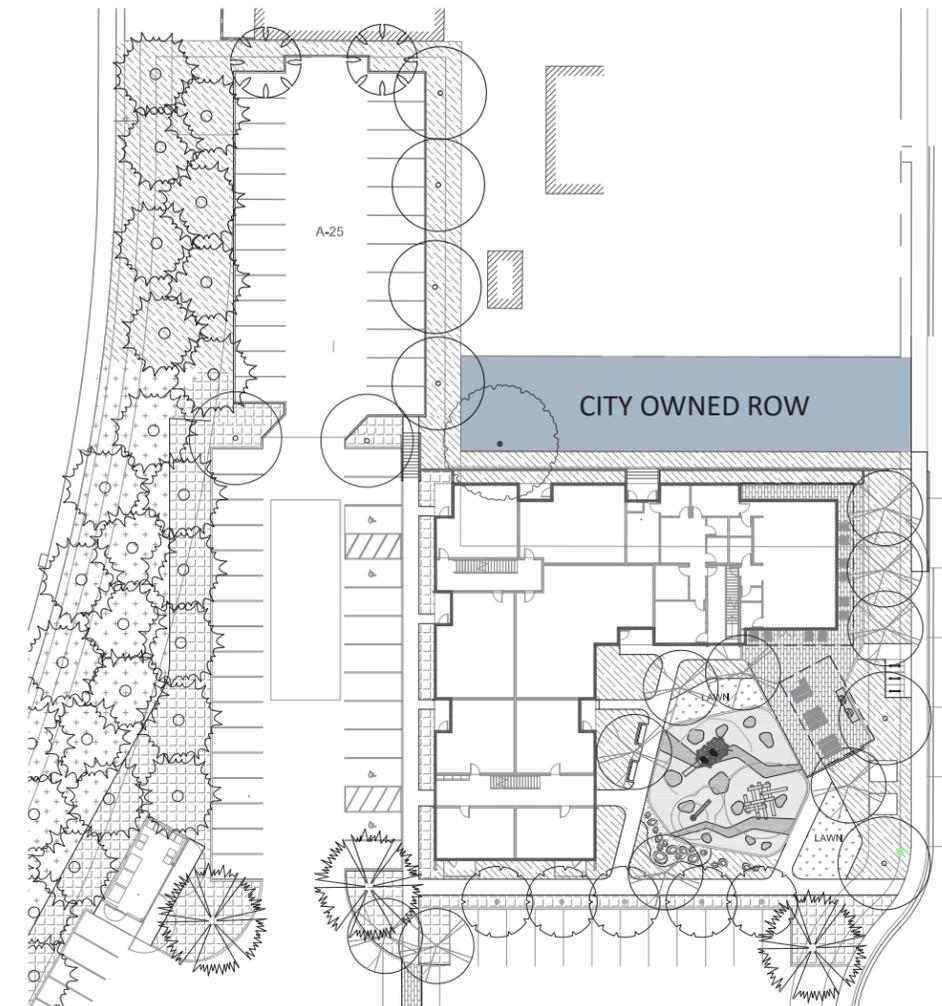
PROPOSED OPTION A

- Future bridge landing accommodated
- Pedestrian/bike path with landscaping
- Retain existing tree buffer on north property line



OPTION B

- Connection to 257th
- Pedestrian/bike path with landscaping
- Retain existing tree buffer on north property line



OPTION C

- Can increase parking ratio from 1.38 to 1.65

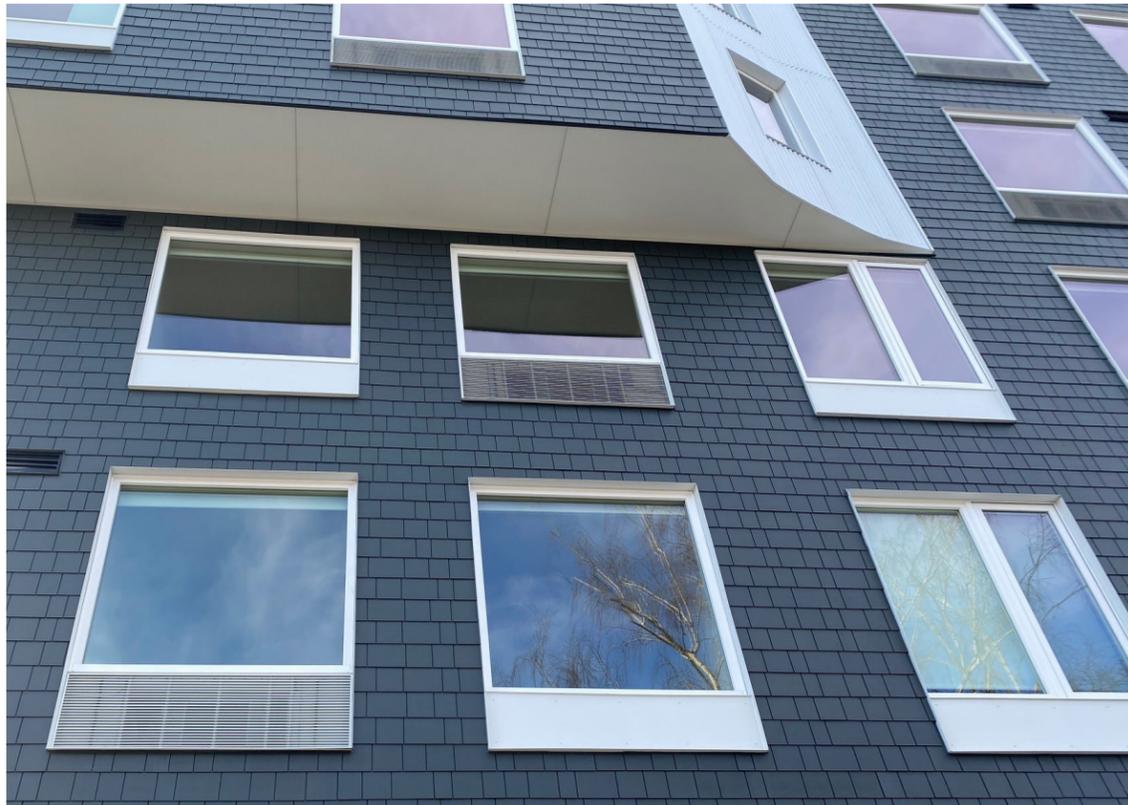
SITE OPTIONS AT SW 2ND STREET RIGHT OF WAY



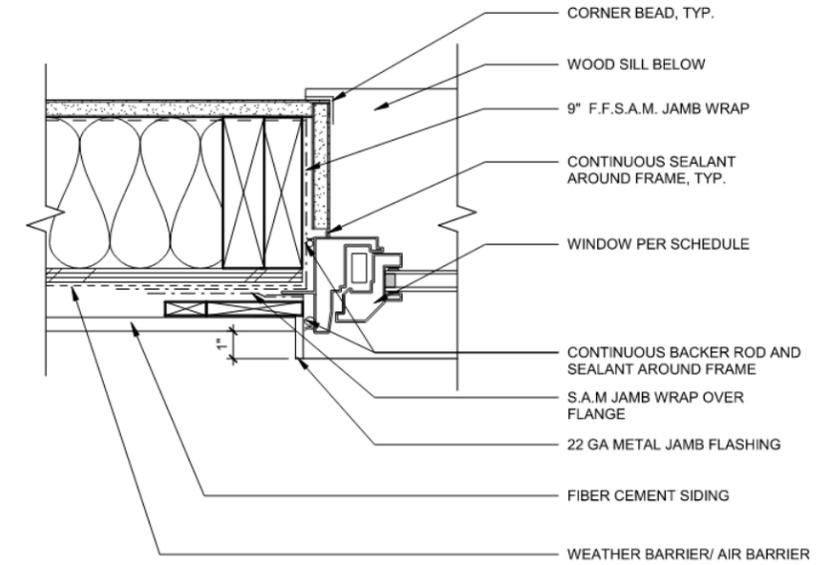
1 - CURRENT PROJECT MASSING - BIRD'S EYE VIEW FROM THE NORTH



14 - VIEW FROM 257TH



NOTE: SEAL ALL FLASHING INTEGRATIONS AND W.R.B. TO MAINTAIN AIR BARRIER CONTINUITY

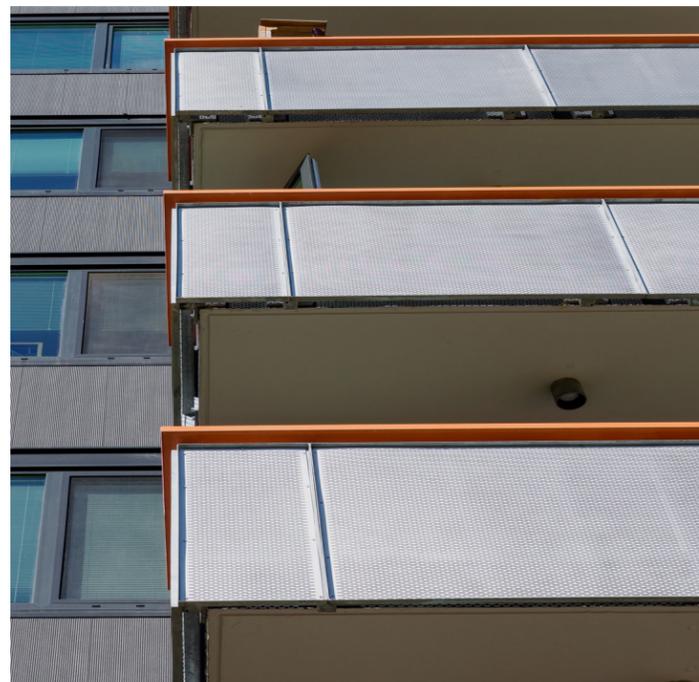


2 WINDOW JAMB AT FIBER CEMENT TYP.
SCALE: 3" = 1'-0"

PRECEDENTS: FIBER CEMENT SHINGLE SIDING WITH PROTRUDING METAL WINDOW TRIM



BLACK VINYL WINDOWS



PERFORATED METAL BALCONY GUARDRAILS



MATERIALS BOARD

PRELIMINARY



REVISION NO. DATE

REVISION NO.	DATE



3 BUILDING A - EAST ELEVATION FROM KENDALL
SCALE:



1 BUILDING A - NORTH EAST CORNER FROM KENDALL
SCALE:



4 BUILDING A - COURTYARD 2 VIEW FROM KENDALL
SCALE:



2 BUILDING A - WEST ELEVATION FROM PARKING LOT A
SCALE:

TROUTDALE HOUSING

SW KENDALL CT &
 SW 2ND ST
 TROUTDALE, OR 97060
 ISSUANCE
 LAND USE APPLICATION
 DATE
 05/31/22
 SCALE

PROJECT NUMBER
 202109.00
 DRAWN BY
 JS
 DRAWING TITLE
**BUILDING A -
 PERSPECTIVES**

SHEET NUMBER
G2.00

PRELIMINARY



REVISION NO. DATE

REVISION NO.	DATE



3 BUILDING B & C - WEST LOOKING INTO COURTYARD 1
SCALE:



1 BUILDING B - EAST ELEVATION FROM PARKING LOT B
SCALE:



4 BUILDING B - NORTH PASS THROUGH
SCALE:



2 BUILDINGS ABC - FROM SOUTH EAST LOOKING NORTH
SCALE:

TROUTDALE HOUSING

SW KENDALL CT &
 SW 2ND ST
 TROUTDALE, OR 97060
 ISSUANCE
 LAND USE APPLICATION
 DATE
 05/31/22
 SCALE

PROJECT NUMBER
 202109.00
 DRAWN BY
 JS
 DRAWING TITLE
**BUILDING B -
 PERSPECTIVES**

SHEET NUMBER
G2.01

PRELIMINARY



REVISION NO. DATE

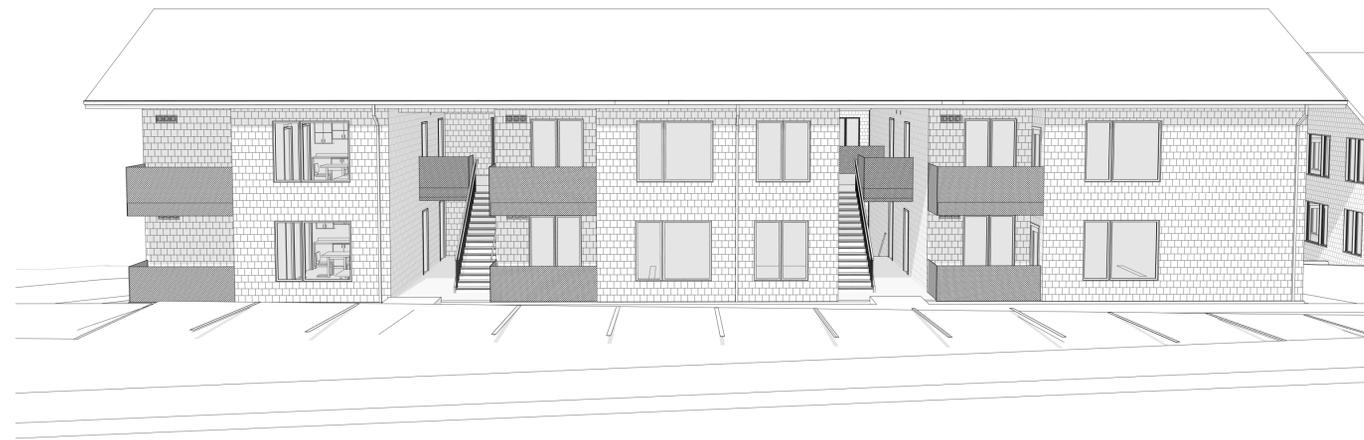
REVISION NO.	DATE



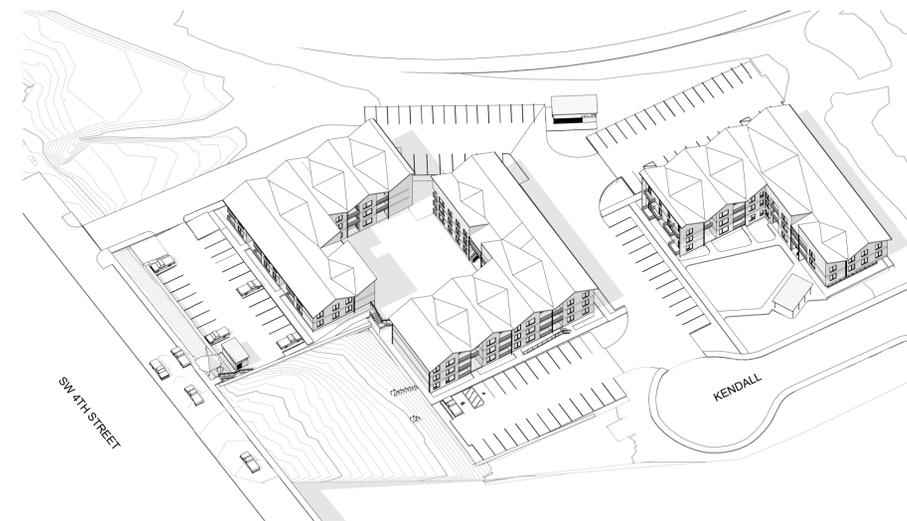
3 BUILDING C - SOUTH EAST ELEVATIONS
 SCALE:



1 BUILDING C - COURTYARD 1 NORTH
 SCALE:



4 BUILDING C - SOUTH ELEVATION FROM PARKING LOT C
 SCALE:



2 BUILDINGS ABC SOUTH EAST AXON
 SCALE:

TROUTDALE HOUSING

SW KENDALL CT &
 SW 2ND ST
 TROUTDALE, OR 97060
 ISSUANCE
 LAND USE APPLICATION
 DATE
 05/31/22
 SCALE

PROJECT NUMBER
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 DRAWN BY
 JS
 DRAWING TITLE
**BUILDING C -
 PERSPECTIVES**

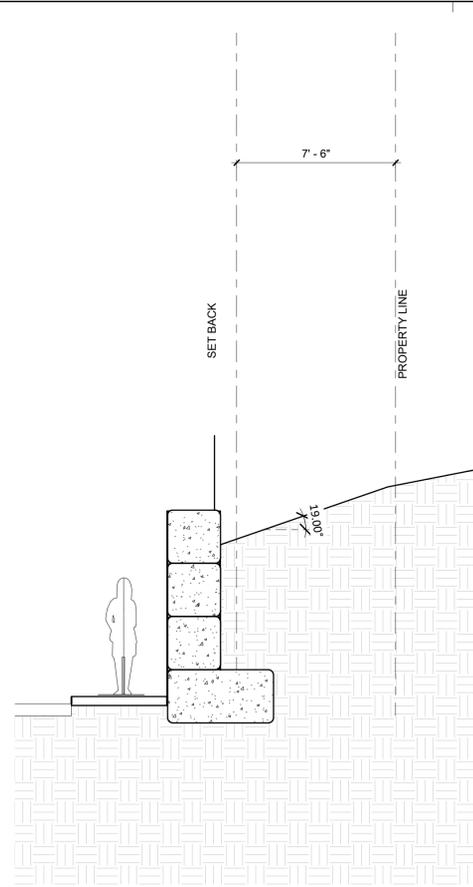
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PRELIMINARY

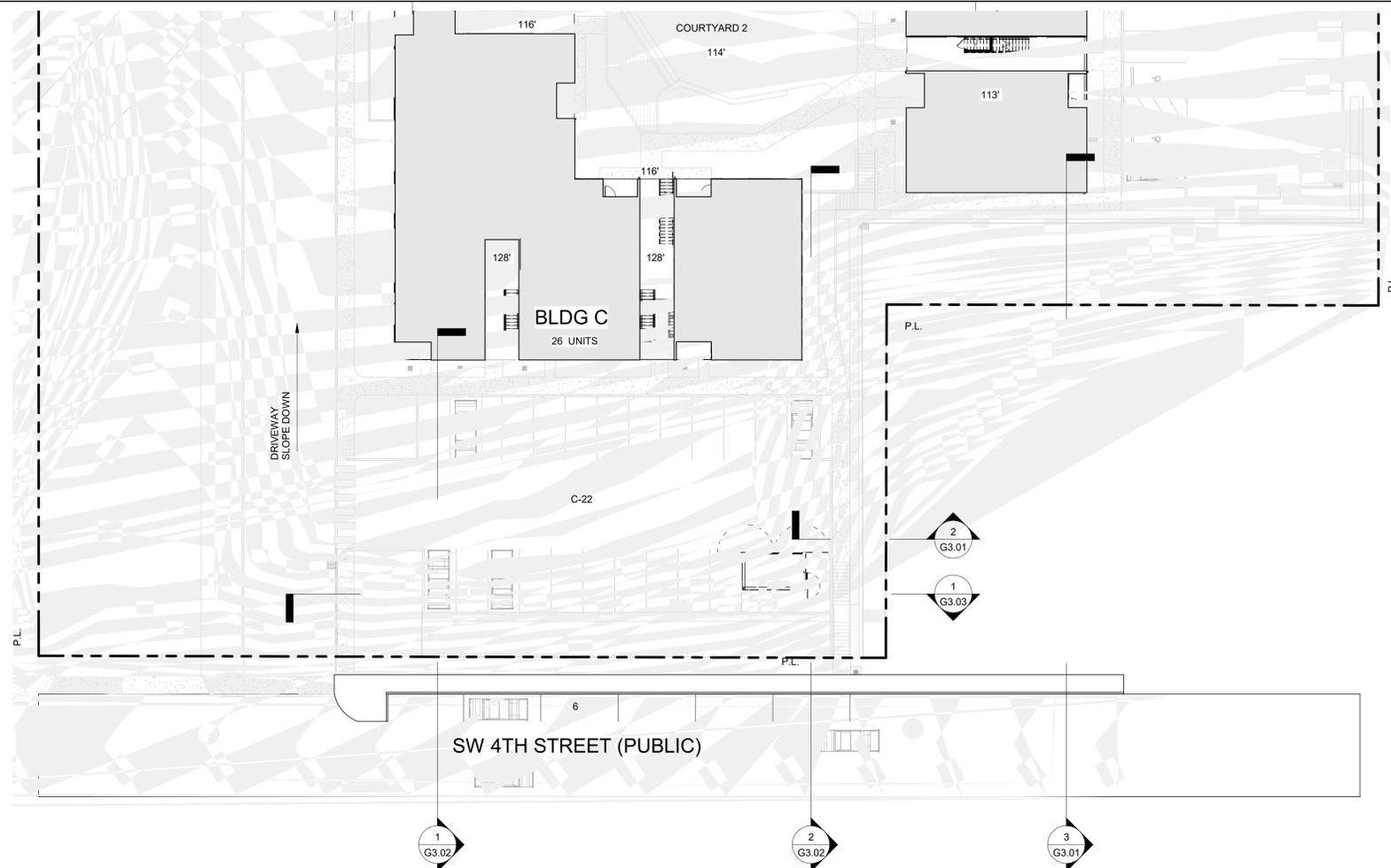


REVISION NO. DATE

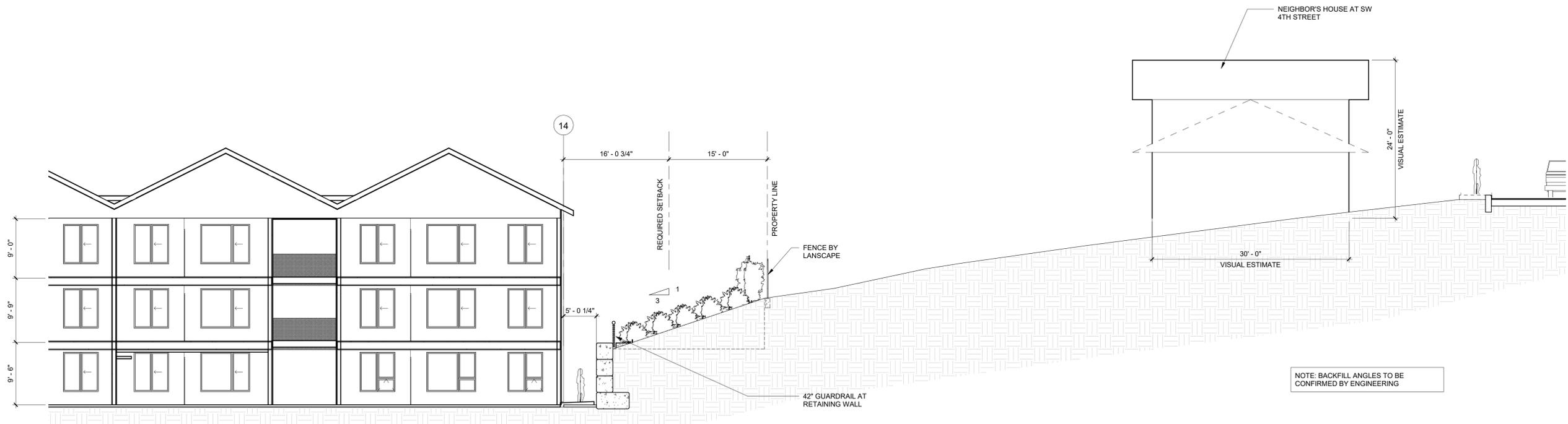
REVISION NO.	DATE



2 SECTION AT NORTH SOUTH RETAINING WALL
 SCALE: 1/4" = 1'-0"



1 ARCHITECTURAL SITE PLAN SHORT
 SCALE: 3/64" = 1'-0"



3 SITE SECTION THROUGH BUILDING B, SOUTHEAST RETAINING WALL, NEIGHBOR'S PROPERTY
 SCALE: 1/8" = 1'-0"

TROUTDALE HOUSING

SW KENDALL CT &
 SW 2ND ST
 TROUTDALE, OR 97060

ISSUANCE
 LAND USE APPLICATION

DATE
 05/31/22

SCALE
 As indicated

PROJECT NUMBER
 202109.00

DRAWN BY
 JS

DRAWING TITLE
SITE SECTIONS

SHEET NUMBER

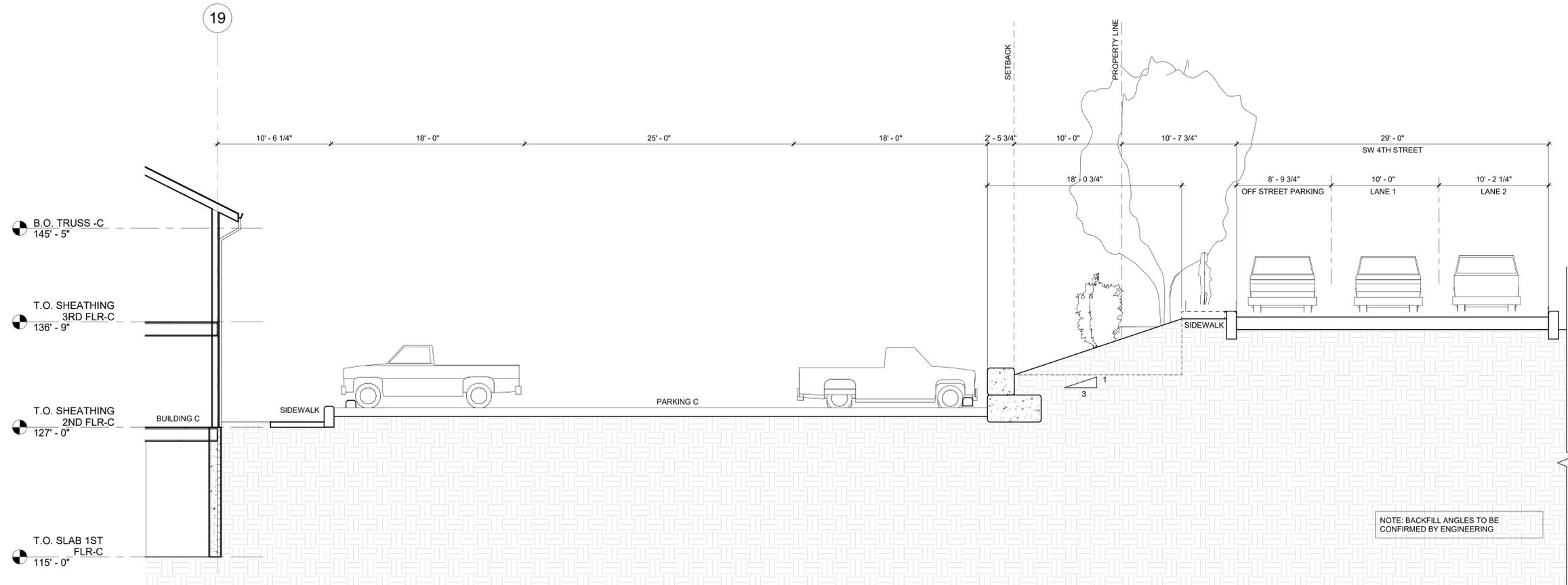
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PRELIMINARY

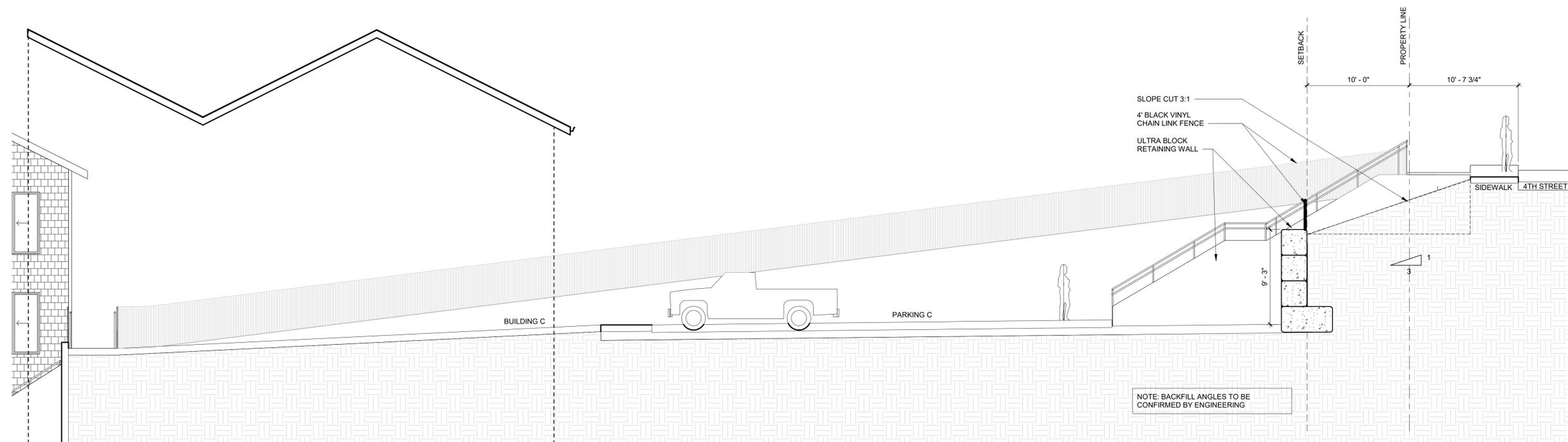


REVISION NO. DATE

REVISION NO.	DATE



1 SITE SECTION AT BUILDING C, PARKING C, SOUTHWEST WALL, SW 4TH STREET
 SCALE: 3/16" = 1'-0"



2 SITE SECTION AT BUILDING C, PARKING C, WEST WALL, SW 4TH STREET
 SCALE: 3/16" = 1'-0"

TROUTDALE HOUSING

SW KENDALL CT &
 SW 2ND ST
 TROUTDALE, OR 97060
 ISSUANCE
 LAND USE APPLICATION
 DATE
 05/31/22
 SCALE
 3/16" = 1'-0"
 PROJECT NUMBER
 202109.00
 DRAWN BY
 JS
 DRAWING TITLE
SITE SECTIONS

SHEET NUMBER

G3.02

PRELIMINARY



REVISION NO. DATE

TROUTDALE HOUSING

SW KENDALL CT &
 SW 2ND ST
 TROUTDALE, OR 97060

ISSUANCE
 LAND USE APPLICATION

DATE
 05/31/22

SCALE
 3/16" = 1'-0"

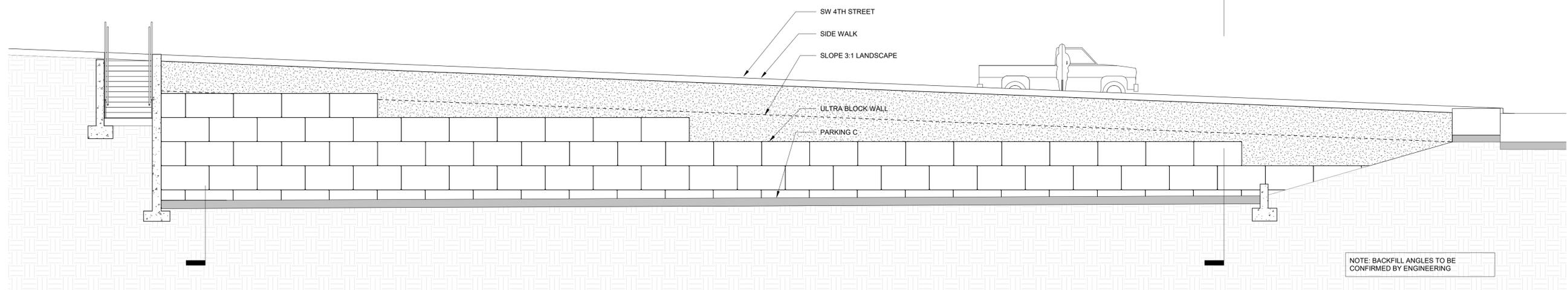
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DRAWN BY
 JS

DRAWING TITLE
SITE SECTIONS

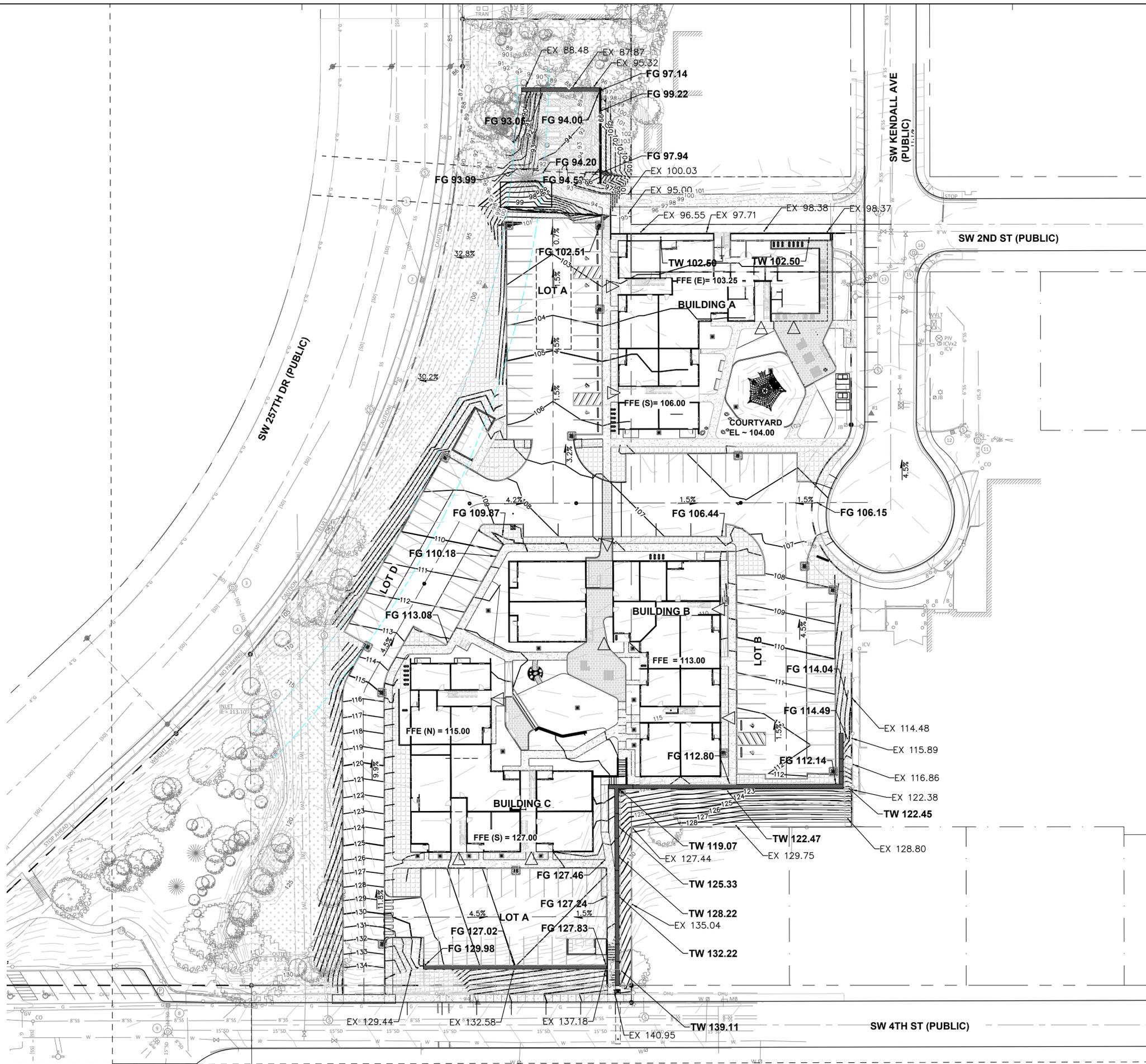
SHEET NUMBER

G3.03



1 PARKING C SOUTH WALL ALONG 4TH STREET
 SCALE: 3/16" = 1'-0"

1/10/2022 11:18:49 AM



mwa architects
 70 NW COUCH STREET
 SUITE 401
 PORTLAND, OR 97209
 P 503 973 5151
 F 503 973 5060
 MWAARCHITECTS.COM



EMERIO
 ENGINEERING • SURVEYING • DESIGN

REVISION NO	DATE

TROUTDALE HOUSING

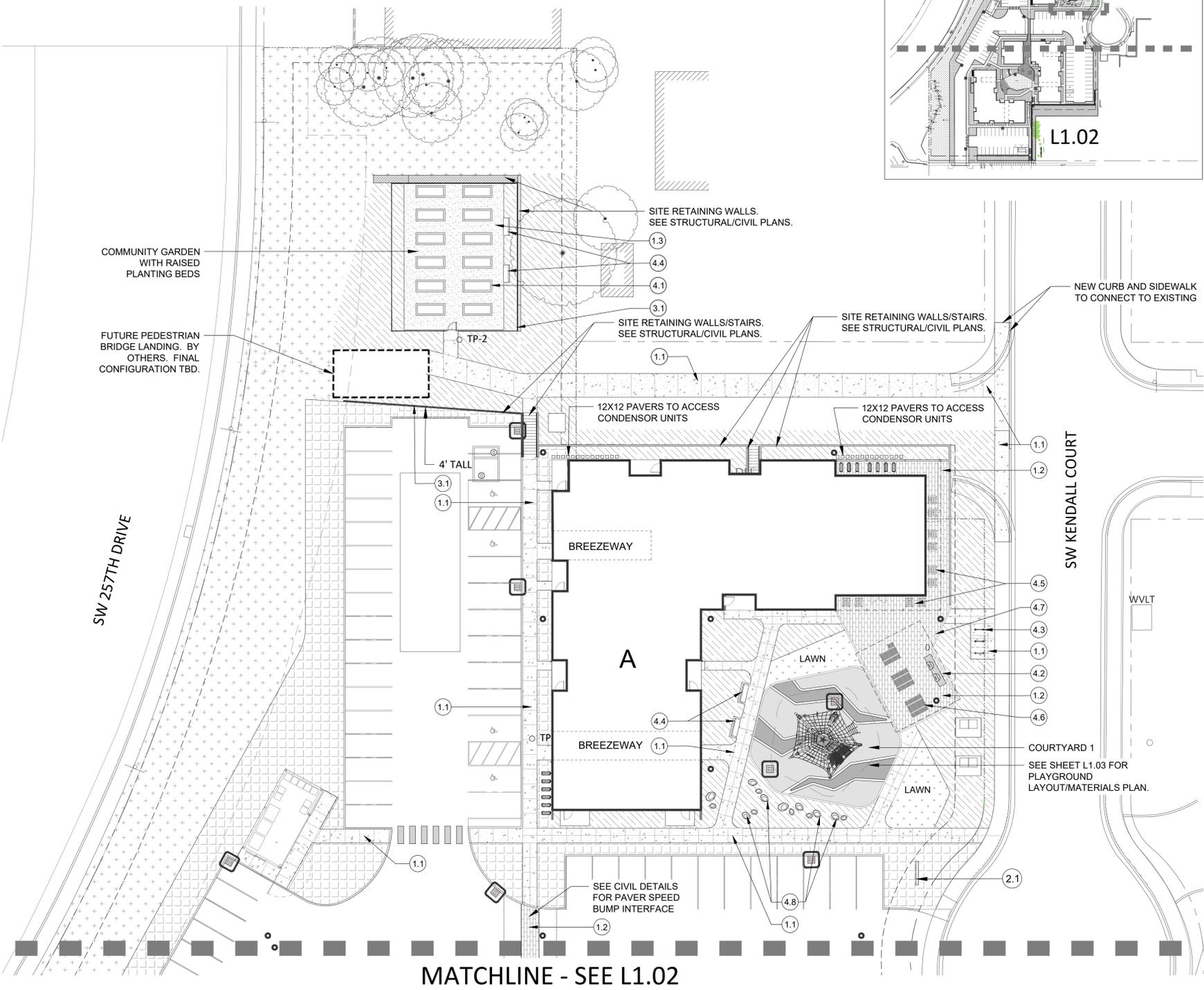
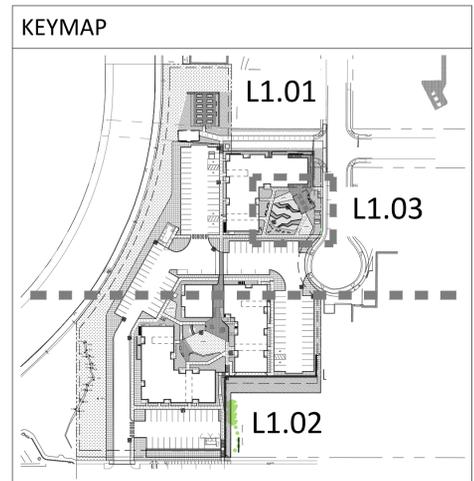
SW KENDALL CT &
 SW 2ND ST
 TROUTDALE, OR 97060
 ISSUANCE
 LAND USE APPLICATION
 DATE
 05/31/22
 SCALE

PROJECT NUMBER
 202109.00
 DRAWN BY

DRAWING TITLE
OVERALL SITE GRADING PLAN

SHEET NUMBER

C2.00



MATERIALS LEGEND

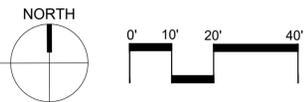
	PROPERTY LINE
	SCORED PEDESTRIAN CONCRETE PAVING
	DECOMPOSED GRANITE PAVING
	1X4 LARGE FORMAT PAVERS
	CONCRETE SEATWALLS/RETAINING WALLS
	BIKE RACKS
	SITE FURNITURE
	BOULDERS SOUTH OF PLAYGROUND
	JUNIPER RAISED PLANTER BOXES
	BARBECUE
	GENERAL PLANTINGS
	PARKING LOT PLANTINGS
	RESTORED HILLSIDE PLANTINGS
	SEEDED LAWN

SITE DETAIL KEYNOTES

KEYNOTE	DESCRIPTION	QTY.	DETAIL/SHEET	SPEC SECTION
1.0	PAVING			
	1.1	CONCRETE PAVING - PEDESTRIAN	11,965 S.F.	2+3/L5.07 32 13 13
	1.2	LARGE FORMAT PAVERS WITH CURB/CURB RAMP	3,770 S.F.	4/L5.07 32 14 13
	1.3	DECOMPOSED GRANITE PAVING	1,671 S.F.	5/L5.07 32 15 40
2.0	RETAINING WALLS / SEATWALLS / ENTRYWAY SIGNAGE			
	2.1	ENTRYWAY SIGNAGE	1	SEE ARCH PLANS SEE ARCH PLANS
	2.2	CONCRETE SEATWALL WITH WOOD SEATBOARDS	35 L.F.	6/L5.03 03 30 10
	2.3	CONCRETE STAIRS WITH RAIL	10 S.F. + 5 L.F.	2/L5.04 03 30 10 05 50 00
	2.4	CONCRETE RAMP WITH RAIL	100 S.F. + 24 L.F.	1/L5.05 03 30 10 05 50 00
	2.5	CONCRETE RAMP AND STAIRS WITH RAIL	177 S.F. + 5 L.F.	1/L5.04 03 30 10 05 50 00
	2.6	LONG CONCRETE STAIRS WITH RAILS	82 S.F. + 14 L.F.	2/L5.02 03 30 10 05 50 00
3.0	FENCING / RAILS			
	3.1	4-6' TALL ORNAMENTAL GUARDRAIL/FENCE/GATE	541 L.F.	2+3/L5.06 32 31 13
	3.2	6' CHAINLINK FENCE WITH SLATS	393 L.F.	1/L5.06 32 31 19
	3.3	TYPICAL RAIL DETAIL	133 L.F.	2/L5.05 05 50 00
4.0	SITE FURNITURE			
	4.1	4X8 STACKED JUNIPER RAISED PLANTERS	12	1/L5.07 06 10 63
	4.2	CUSTOM ELECTRIC DOUBLE BBQ	1	1/L5.02 12 93 00
	4.3	BIKE RACKS	5	4/L5.03 12 93 00
	4.4	SITE BENCHES	7	1/L5.03 12 93 00
	4.5	LOUNGE CHAIRS	10	3/L5.03 12 93 00
	4.6	SITE TABLES	5	2/L5.03 12 93 00
	4.7	TRASH RECEPTACLE	2	5/L5.03 12 93 00
	4.8	SITE BOULDERS	14	5/L5.01 12 93 00
5.0	PLAYGROUND			
5.1	NATURE PLAYGROUND		SEE L1.03 11 68 00	

- LANDSCAPE NOTES**
- FOR GRADING, UTILITIES AND DRAINAGE, AND PARKING LOT SEE CIVIL PLANS.
 - FOR SIGHT LIGHTING SEE LIGHTING CONSULTANTS PLANS AND NARRATIVES.
 - FOR RETAINING WALL, MAJOR RAMP AND MAJOR STAIR DETAILS, SEE STRUCTURAL.
 - FOR SITE SIGNAGE SEE ARCH PLANS.
 - SEE CIVIL PLANS FOR SLOPE STABILIZATION METHODS.
 - ALL INVASIVE PLANTS TO BE REMOVED FROM THE HILLSIDE FOR THE BASE BID.
 - STAIRS LEADING TO PEDESTRIAN BRIDGE ARE DRAWN CONCEPTUALLY. STAIRS WILL BE MODIFIED ONCE THE SCOPE OF WORK IS KNOWN.

01 LAYOUT / MATERIALS PLAN
 1" = 20'-0"





MATERIALS LEGEND

	SCORED PEDESTRIAN CONCRETE PAVING
	ARTIFICIAL PLAY TURF - COLORS TBD
	POURED IN PLACE PLAY SURFACING: MAIN COLOR BORDER COLOR
	LARGE FORMAT PAVERS
	GENERAL PLANTINGS
	SEEDED LAWN

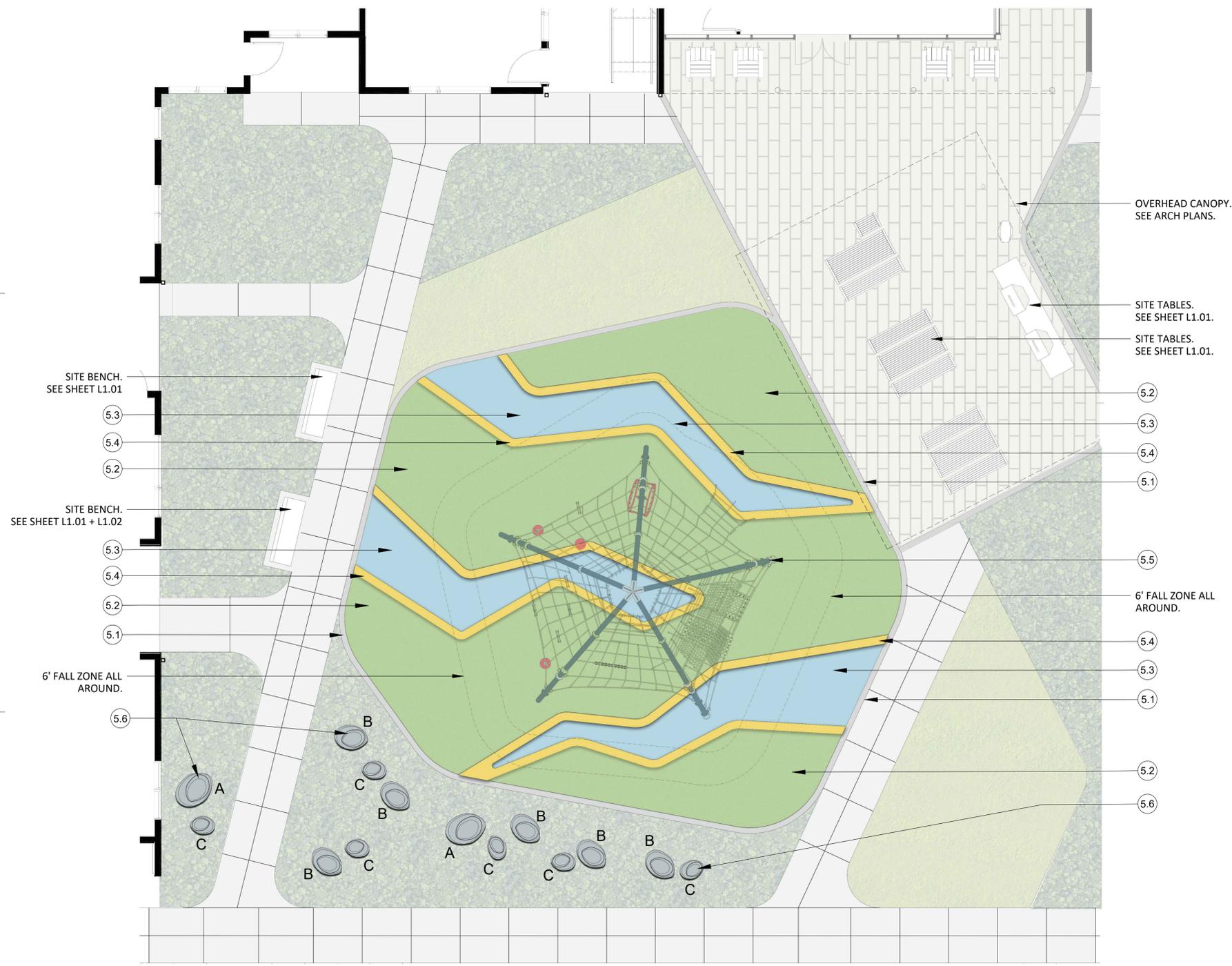
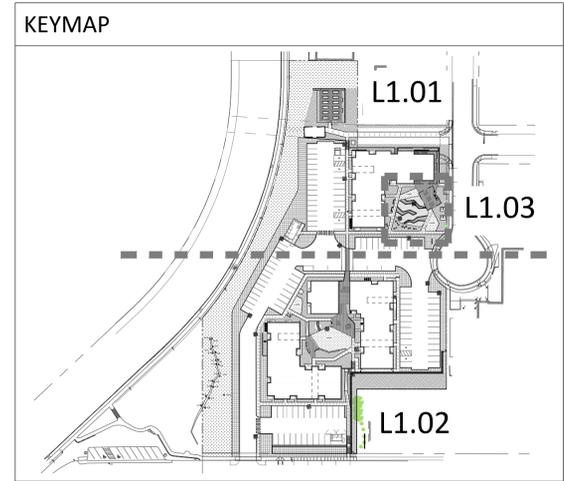
SITE DETAIL KEYNOTES

5.0	PLAYGROUND	QTY.	DTL/SHEET	SPEC SECTION
5.1	CONCRETE CURB	149 L.F.	4/L5.01	11 68 00
5.2	ARTIFICIAL TURF	1,233 S.F.	2/L5.01	11 68 00
5.3	PIP SURFACING - PRIMARY COLOR	378 S.F.	3/L5.01	11 68 00
5.4	PIP SURFACING - BORDER COLOR	176 S.F.	3/L5.01	11 68 00
5.5	SENSORY DOME	1	1/L5.01	11 68 00
5.6	BOULDERS	14	5/L5.01	11 68 00

BOULDER LEGEND

A.	LARGE BOULDER AT EVENT CIRCLE - 36-48" L X 36-48" W X 30".
B.	MEDIUM BOULDERS - 30-42" L X 30-42" W X 24" HIGH
C.	SMALL BOULDERS - 24-36" L X 24-36" W X 18-24" HIGH

- LANDSCAPE NOTES**
- GRADES WILL UNDULATE SOME TO ADD TO THE SENSORY EXPERIENCE. GRADES STILL TO BE DETERMINED.
 - FOR GRADING, UTILITIES AND DRAINAGE, SEE CIVIL PLANS.
 - A SHOP DRAWING WILL BE PROVIDED BY INSTALLING CONTRACTOR TO LANDSCAPE ARCHITECT FOR APPROVAL. SHOP DRAWING WILL INCLUDED BUT NOT BE LIMITED TO DIMENSIONS SHOWING ORIENTATION OF PLAY EQUIPMENT, DIMENSIONS OF PLAY SURFACING OUTLINES, FOOTING PLACEMENTS, FALL ZONE DIMENSIONS, ETC..
 - FOR EQUIPMENT COLORS, SEE DETAILS AND SPECIFICATIONS.
 - LANDSCAPE ARCHITECT TO APPROVE LAYOUT OF PLAYGROUND CURBS BEFORE INSTALLATION.



01 LAYOUT / MATERIALS PLAN - PLAYGROUND
 3/16" = 1'-0"

NORTH

0' 10' 20' 40'

TROUTDALE HOUSING

SW KENDALL CT &
 SW 2ND ST
 TROUTDALE OR 97060

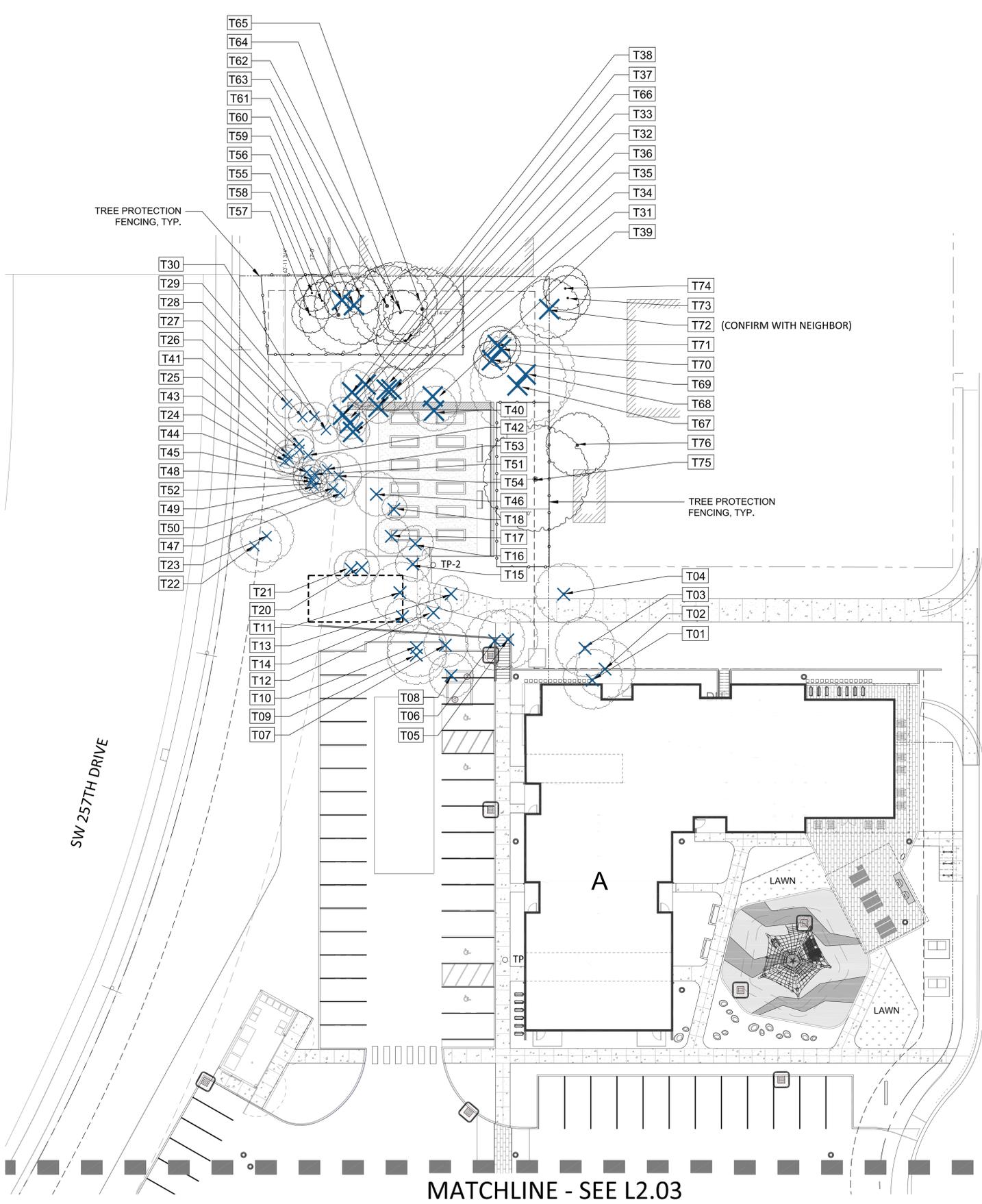
ISSUANCE
 LAND USE APPLICATION
 DATE
 05.31.22
 SCALE
 AS SHOWN
 PROJECT NUMBER
 202109.00
 DRAWN BY
 DE
 DRAWING TITLE
LAYOUT / MATERIALS PLAN
 SHEET NUMBER

L1.03

01 January, 2000

01 TREE PROTECTION / REMOVAL PLAN

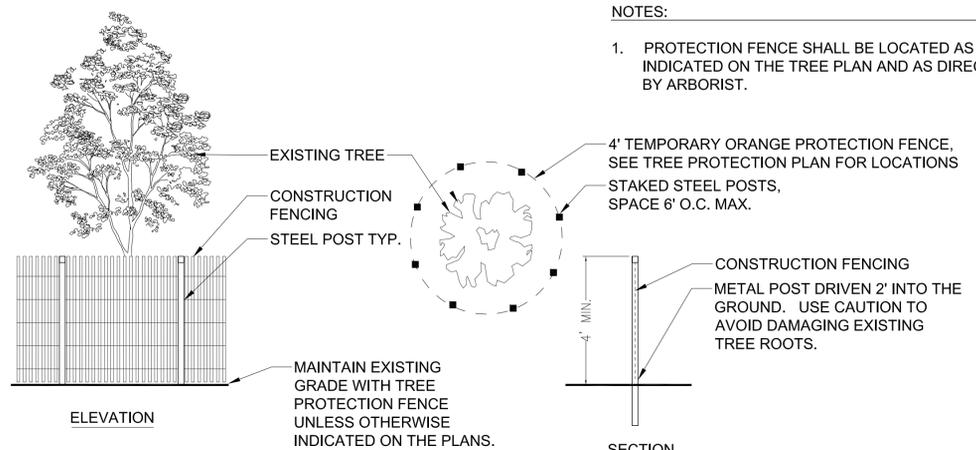
1" = 20'-0"



02 ON SITE TREE PROTECTION FENCING DETAIL

NTS

DETAIL



NOTES:

1. PROTECTION FENCE SHALL BE LOCATED AS INDICATED ON THE TREE PLAN AND AS DIRECTED BY ARBORIST.

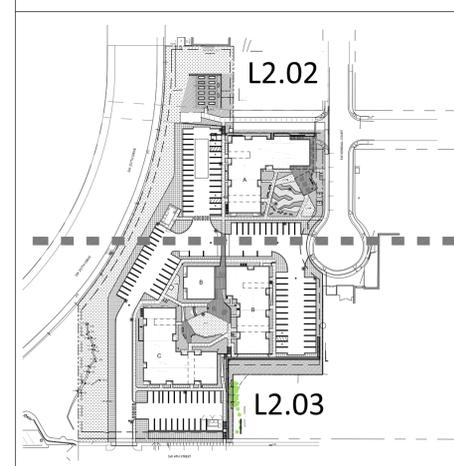
LEGEND

	PROPERTY LINE
	EXISTING TREES TO REMAIN
	EXISTING TREES TO BE REMOVED
	ON SITE TREE PROTECTION FENCING - 3.5' TALL ORANGE TEMPORARY FENCING

TREE PROTECTION / TRANSPLANTING NOTES

1. SEE ACCOMPANYING ARBORIST REPORT FOR MORE INFORMATION.
2. A CURRENTLY QUALIFIED ISA CERTIFIED ARBORIST (PROJECT ARBORIST) WILL BE ON-SITE AT ALL TIMES DURING ANY PAVING INSTALLATION, SURFACE DISTURBANCE OR EXCAVATION WORK WITHIN 25-FT OF PRESERVED TREES.
3. A PRE-CONSTRUCTION MEETING WILL TAKE PLACE BETWEEN THE CONTRACTORS AND THE PROJECT ARBORIST TO DISCUSS TREE PROTECTION PLANNING FOR THE TWO PRESERVED TREES.
4. THE PLACEMENT OF ALL TREE PROTECTION FENCING, AS DETAILED ON THE CONSTRUCTION PLANS, MUST OCCUR BEFORE ANY CONSTRUCTION, EXCAVATION OR STORAGE OF MATERIALS OR EQUIPMENT TAKES PLACE AT THE SITE. THE PROJECT ARBORIST MUST APPROVE THE LOCATION OF THE FENCING BEFORE SITE WORK COMMENCES.
5. TREE PROTECTION FENCING WILL CONSIST OF SECURELY JOINED SECTIONS OF 6-FT TALL CHAIN LINK FENCING SECURED FIRMLY INTO THE GROUND BY METAL POSTS OR REBAR.
6. NO CONSTRUCTION ACTIVITY, INCLUDING VEHICLE ACCESS, OR ANY STORAGE OF SPOIL, MATERIALS OR EQUIPMENT WILL OCCUR WITHIN THE AREA PROTECTED BY THE TREE PROTECTION FENCE UNLESS APPROVED BY THE PROJECT ARBORIST.
7. THE PROTECTION FENCE LOCATIONS AS DETAILED IN THE CONSTRUCTION PLANS WILL NOT BE ALTERED OR BREACHED AT ANY TIME WITHOUT THE EXPLICIT APPROVAL OF THE PROJECT ARBORIST.
8. ALL SEVERED OR BADLY DAMAGED ROOTS OF ANY PRESERVED TREE MUST BE CUT CLEANLY USING HAND-HELD TOOLS (E.G. HAND SAW, RECIPROCATING SAW, CIRCULAR SAW, ANGLE GRINDER OR BY OTHER MEANS APPROVED BY THE PROJECT ARBORIST).
9. THE CRZ, AS DETERMINED BY ARBORIST, MATCHES THE DIAMETER AT BREAST HEIGHT OF EACH TREE LISTED.
10. ANY REQUIRED PRUNING OF THE PRESERVED TREES MUST BE COMPLETED BEFORE INSTALLATION OF THE TREE PROTECTION FENCING AND BEFORE ANY CONSTRUCTION WORK COMMENCES. ALL TREE PRUNING WORK MUST BE COMPLETED BY AN ISA CERTIFIED ARBORIST.
11. A TREE REMOVAL PERMIT SHALL BE OBTAINED BY HOMEFORWARD PER TREE PERMIT REQUIREMENTS AS WRITTEN IN THE CITY OF TROUTDALE CODE, CHAPTER 13.10.
12. ARBORIST TO PROTECT EXISTING LILACS ALONG SOUTHEASTERN PROPERTY LINE AS SHOWN. IF ARBORIST OPINION IS THAT THE TREES CAN'T BE PROTECTED, TRANSPLANT TREES TO A NEARBY LOCATION DECIDED UPON BY ADJACENT RESIDENTIAL PROPERTY OWNER AND HOMEFORWARD.

KEYMAP



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 70 NW COUCH STREET
 SUITE 401
 PORTLAND, OR 97209
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 F 503 973 5060
 MWAARCHITECTS.COM

STAMP
PRELIMINARY

OWNER LOGO

CONSULTANT LOGO

REVISION NO	DATE

TROUTDALE HOUSING

SW KENDALL CT &
 SW 2ND ST
 TROUTDALE OR 97060

ISSUANCE LAND USE APPLICATION

DATE 05.31.22

SCALE AS SHOWN

PROJECT NUMBER 202109.00

DRAWN BY DE

DRAWING TITLE

TREE PLAN

SHEET NUMBER

L2.02

VEHICLE PARKING SUMMARY

9.005 2 SPACES PER DWELLING UNIT = 94 UNITS x 2 = 188 SPACES

	REQUIRED	PROVIDED
ON SITE PARKING	188	119
STANDARD 9' X 18'	-	79
COMPACT 8' X 16' (35% MAX)	-	34
ACCESSIBLE 9' X 18'	-	5 TYP / 1 VAN
ON STREET PARKING	-	11
TOTAL	188 (2.0)	130 (1.38)
PARKING REDUCTION APPROVED PER FILE NO. X		
EV INFRASTRUCTURE CODE OHCS	5% OF 119 = 6 5% OF 119 = 6	12
DRIVEWAYS	25'	25' / 26'

BICYCLE PARKING SUMMARY

9.08 BIKE PARKING (5% ACCESSIBLE)

	REQUIRED	PROVIDED
SHORT TERM:	5% OF 216 = 10	10 EXT
LONG TERM:	3% OF 216 = 6	6 (INT BLDG C)
SIZE:	2'-6" X 6'-0" W/ 5' ACCESS AISLE	2'-6" X 6'-0" W/ 5' ACCESS AISLE

PROJECT CAMPUS DESCRIPTION

TOTAL BUILDING GSF
(*DOES NOT INCLUDE BREEZEWAYS, PATIO/BALCONIES OR CRAWL SPACE)

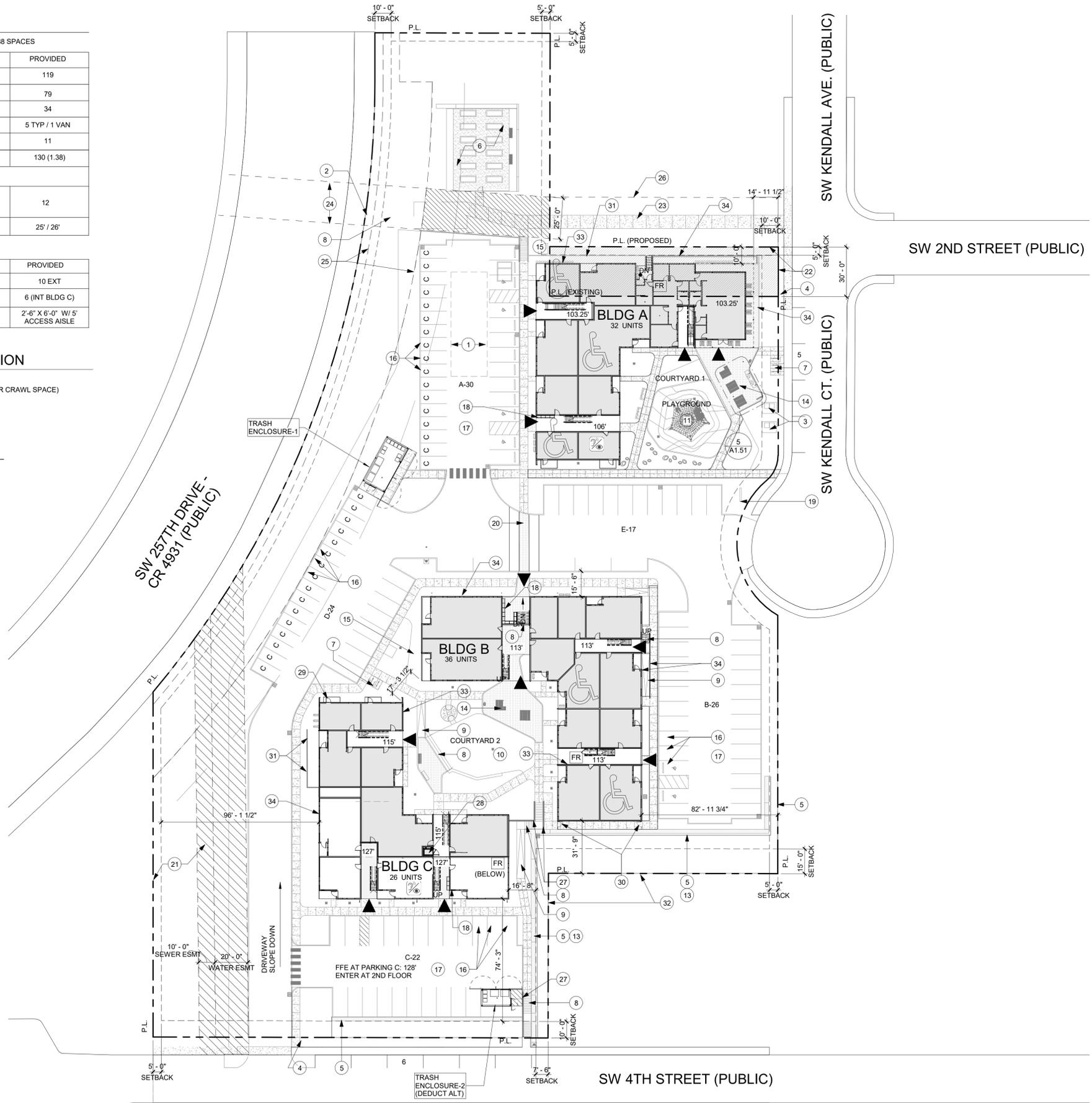
BUILDING A	= 25,331 SF
BUILDING B	= 27,010 SF
BUILDING C	= 22,036 SF
TOTAL	= 74,337 SF

TOTAL BUILDING UNITS: TOTAL UNIT TYPES:

BUILDING A	= 32 UNITS	STUDIO	= 18
BUILDING B	= 36 UNITS	1BR	= 29
BUILDING C	= 26 UNITS	2BR	= 35
TOTAL	= 94 UNITS	3BR	= 12

TOTAL BUILDING AREAS:

BLDG A	= 25,331 SF
BLDG B	= 27,010 SF
BLDG C	= 22,036 SF



1 ARCHITECTURAL SITE PLAN
SCALE: 1/32" = 1'-0"

SHEET GENERAL NOTES

- PROPERTY LINE BASED ON SURVEY BY KPFF.
- SEE CIVIL DRAWINGS FOR UTILITY EASEMENT, SITE DIMENSIONS AT PARKING, SITE UTILITIES, GRADING, EROSION CONTROL, BUILDING PAD ELEVATIONS AND RELATED DETAILS.
- SEE LANDSCAPE DRAWINGS FOR HARDSCAPE DIMENSIONS, PLANTING AND IRRIGATION INFORMATION, AND TREE REMOVAL AND PROTECTION.
- SEE CIVIL AND LANDSCAPE DRAWINGS FOR ACCESSIBILITY SITE RELATED DETAILS.
- SEE ELECTRICAL FOR SITE LIGHTING AND SERVICE DISTRIBUTION.
- DIMENSIONS AT BUILDING ARE TO FACE OF FOUNDATION STEM WALL.
- NOT ALL KEYNOTES OR LEGEND SYMBOLS USED ON EVERY SHEET.

KEYNOTES

- | KEYNOTE | SYMBOL | DESCRIPTION |
|---------|--------|--|
| 1 | (X) | INFILTRATION / DETENTION STORMTECH CHAMBER BELOW, COORDINATE W/ CIVIL |
| 2 | (X) | ACCESS PATH TO 257TH SEE ALT-XX |
| 3 | (X) | WATER AND FIRE VAULTS, COORDINATE W/ CIVIL |
| 4 | (X) | FIRE HYDRANT, COORDINATE W/ CIVIL |
| 5 | (X) | ULTRABLOCK RETAINING WALL, COORDINATE W/ CIVIL, LANDSCAPE & STRUCT |
| 6 | (X) | COMMUNITY GARDEN W/ FENCE & GATES, COORDINATE W/ LANDSCAPE |
| 7 | (X) | STAPLE BIKE RACK, COORDINATE W/ LANDSCAPE |
| 8 | (X) | STAIRS AND HANDRAILS, COORDINATE W/ CIVIL AND LANDSCAPE |
| 9 | (X) | RAMP AND HANDRAILS, COORDINATE W/ CIVIL AND LANDSCAPE |
| 10 | (X) | LAWN COURTYARD W/ SEAT WALL, COORDINATE W/ LANDSCAPE |
| 11 | (X) | PLAYGROUND, COORDINATE W/ LANDSCAPE |
| 12 | (X) | HARDSCAPE PATIO / BBQ AREA, COORDINATE W/ LANDSCAPE |
| 13 | (X) | 42" TALL WROUGHT IRON FENCE AT RETAINING WALL, COORDINATE W/ LANDSCAPE |
| 14 | (X) | SITE FURNISHINGS, COORDINATE W/ LANDSCAPE |
| 15 | (X) | PROPOSED LOCATION OF TRANSFORMER, COORDINATE W/ ELECTRICAL |
| 16 | (X) | LOCATION OF FUTURE DUAL ELECTRIC VEHICLE CHARGERS, (12) TOTAL, COORDINATE W/ ELECTRICAL |
| 17 | (X) | SITE LIGHTING, COORDINATE W/ ELECTRICAL |
| 18 | (X) | CLUSTER MAIL BOX UNITS |
| 19 | (X) | MONUMENT SIGN, PROVIDE POWER FOR ILLUMINATION. (UNDER SEPERATE PERMIT) |
| 20 | (X) | SPEED HUMP RAISED PEDESTRIAN CROSSWALK |
| 21 | (X) | DITCH AREA - NO WORK |
| 22 | (X) | PROPOSED PROPERTY LINE LOCATION 30' NORTH OF EXISTING PROPERTY LINE WITH 1/2 STREET VACATION AT 2ND STREET ROW |
| 23 | (X) | PEDESTRIAN / BIKE PATH |
| 24 | (X) | LOCATION OF EASEMENT FOR POTENTIAL FUTURE BRIDGE ABOVE, BY OTHERS. FINAL LOCATION OF BRIDGE TBD |
| 25 | (X) | EXISTING EASEMENTS, SEE SURVEY |
| 26 | (X) | EXISTING ROW BOUNDARY LINE AT 2ND STREET, SEE SURVEY |
| 27 | (X) | CONCRETE RETAINING WALL COORDINATE W/ STRUCTURAL AND CIVIL |
| 28 | (X) | CONVENIENCE PLATFORM LIFT AT BUILDING C, COORDINATE W/ ELECTRICAL |
| 29 | (X) | BOLT ON BALCONIES |
| 30 | (X) | 6' TALL CHAIN LINK FENCE AND GATE, COORDINATE W/ LANDSCAPE |
| 31 | (X) | CONCRETE RETAINING WALL PLANTERS, COORDINATE W/ CIVIL, LANDSCAPE AND STRUCTURAL |
| 32 | (X) | 6' TALL CHAIN LINK FENCE, COORDINATE W/ LANDSCAPE |
| 33 | (X) | ELECTRICAL METER STACK LOCATION, COORDINATE W/ELECTRICAL |
| 34 | (X) | ARCHITECTURAL CONCRETE FINISH REQUIRED AT THIS LOCATION. BOARD FORM FORM LINER, SEE ELEVATIONS |

LEGEND

- PROPERTY LINE
- SETBACK LINE
- EASEMENT LINE
- LOCATION OF ACCESSIBLE PARKING SPACE / UNIT
ACCESSIBLE UNITS ONLY OCCUR AT GRADE LEVEL
- MAIN BUILDING ENTRY
- FIRE RISER ROOM
- COMPACT PARKING SPACE
- AUDIO / VISUAL UNIT

PRELIMINARY

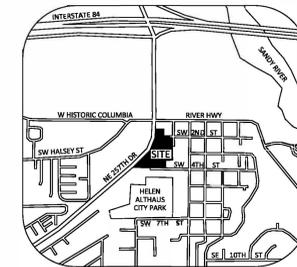


REVISION NO	DATE

TROUTDALE HOUSING

SW KENDALL CT &
SW 2ND ST
TROUTDALE, OR 97060
ISSUANCE
LAND USE APPLICATION
DATE
05/31/22
SCALE
As indicated
PROJECT NUMBER
202109.00
DRAWN BY
JS
DRAWING TITLE
ARCHITECTURAL
SITE PLAN
SHEET NUMBER

A1.00



VICINITY MAP
NOT TO SCALE

NOTES:

- 1.) BASIS OF BEARINGS FOR THIS SURVEY IS THE OREGON COORDINATE REFERENCE SYSTEM (OCRS), PORTLAND ZONE. THE RESULTANT BEARING OF THE CENTERLINE OF SW 4TH STREET IS NORTH 89°51'39" WEST.
- 2.) THE SUBJECT PROPERTY IS OWNED BY MULTNOMAH COUNTY.
- 3.) BOUNDARY AND EASEMENTS SHOWN HEREON ARE BASED ON:
- LOT BOOK SERVICE ORDER NO. NCS-1075179-0R1 BY FIRST AMERICAN TITLE INSURANCE COMPANY, DATED JUNE 21, 2021.
- PER LOT BOOK SERVICE ORDER NO. NCS-1075179-0R1 BY FIRST AMERICAN TITLE INSURANCE COMPANY, DATED JUNE 21, 2021.
- 4.) PROPOSED PARCEL GROSS LAND AREA: 156,014 SQ. FT. (3.582 ACRES)
- 5.) THE SUBJECT PROPERTY SHOWN HEREON APPEARS TO LIE WITHIN ZONE "X", OTHER AREAS, "AREA OF MINIMAL FLOOD HAZARD", PER FLOOD INSURANCE RATE MAP NUMBER 43051C0217, EFFECTIVE DATE: FEBRUARY 1, 2019.
- 6.) UTILITY LOCATIONS SHOWN ARE PER FIELD LOCATED UTILITY PAINT MARKS & REFERENCE MAPS MADE AVAILABLE BY THE VARIOUS UTILITY PROVIDERS. UNLESS INDICATED, DEPTHS OF UTILITY LINES ARE NOT AVAILABLE. ALL UTILITY LOCATIONS SHOULD BE FIELD VERIFIED (POTHOLED) PRIOR TO CONSTRUCTION.

LEGEND:

- BUILDING OUTLINE WITH DOOR
- CONCRETE SURFACE
- ASPHALT SURFACE
- WALL
- BUILDING OVERHANG
- CURB LINE
- EDGE OF ASPHALT
- RIGHT-OF-WAY LINE
- EASEMENT LINE
- LOT LINE
- PROPOSED PROPERTY LINE
- FIBER OPTIC LINE
- ELECTRICAL LINE
- TELECOMMUNICATIONS LINE
- STORM LINE
- SANITARY SEWER LINE
- WATER LINE
- GAS LINE
- OVERHEAD UTILITY LINES
- UNDERGROUND LINE PER AS-BUILTS
- SIGN
- BOLLARD
- DRIVEWAY ENTRY
- HANDICAP RAMP
- MAILBOX
- PLANTER BOX
- ELECTRICAL JUNCTION BOX
- ELECTRICAL METER
- GATE KEYPAD
- ELECTRICAL VAULT
- TRANSFORMER
- GUY ANCHOR
- LUMINAIRE
- POWER POLE
- OVERHEAD LIGHT
- POWER POLE/OVERHEAD LIGHT
- GAS VALVE
- SANITARY MANHOLE WITH STRUCTURE
- STORM MANHOLE WITH STRUCTURE
- CATCH BASIN
- SANITARY/STORM CLEAN OUT
- TELECOMMUNICATIONS VAULT
- TELECOMMUNICATIONS RISER
- TRAFFIC SIGNAL BOX
- WATER VALVE
- FIRE HYDRANT
- WATER METER
- POST INDICATOR VALVE
- FIRE DEPARTMENT CONNECT
- WATER VAULT
- HOSE BIB
- IRRIGATION CONTROL VALVE
- PLANTER BOX
- TRASH CAN
- DECIDUOUS TREE
-PERIMETER REPRESENTS DRIPLINE
- CONIFEROUS TREE
-PERIMETER REPRESENTS DRIPLINE
- SURVEY NUMBER
- PARTITION PLAT



REGISTERED PROFESSIONAL LAND SURVEYOR

OREGON ONE 30, 1987
TROY T. TETSUKA
2841
RENEWAL 6/30/2022

JOB NO. 2100267

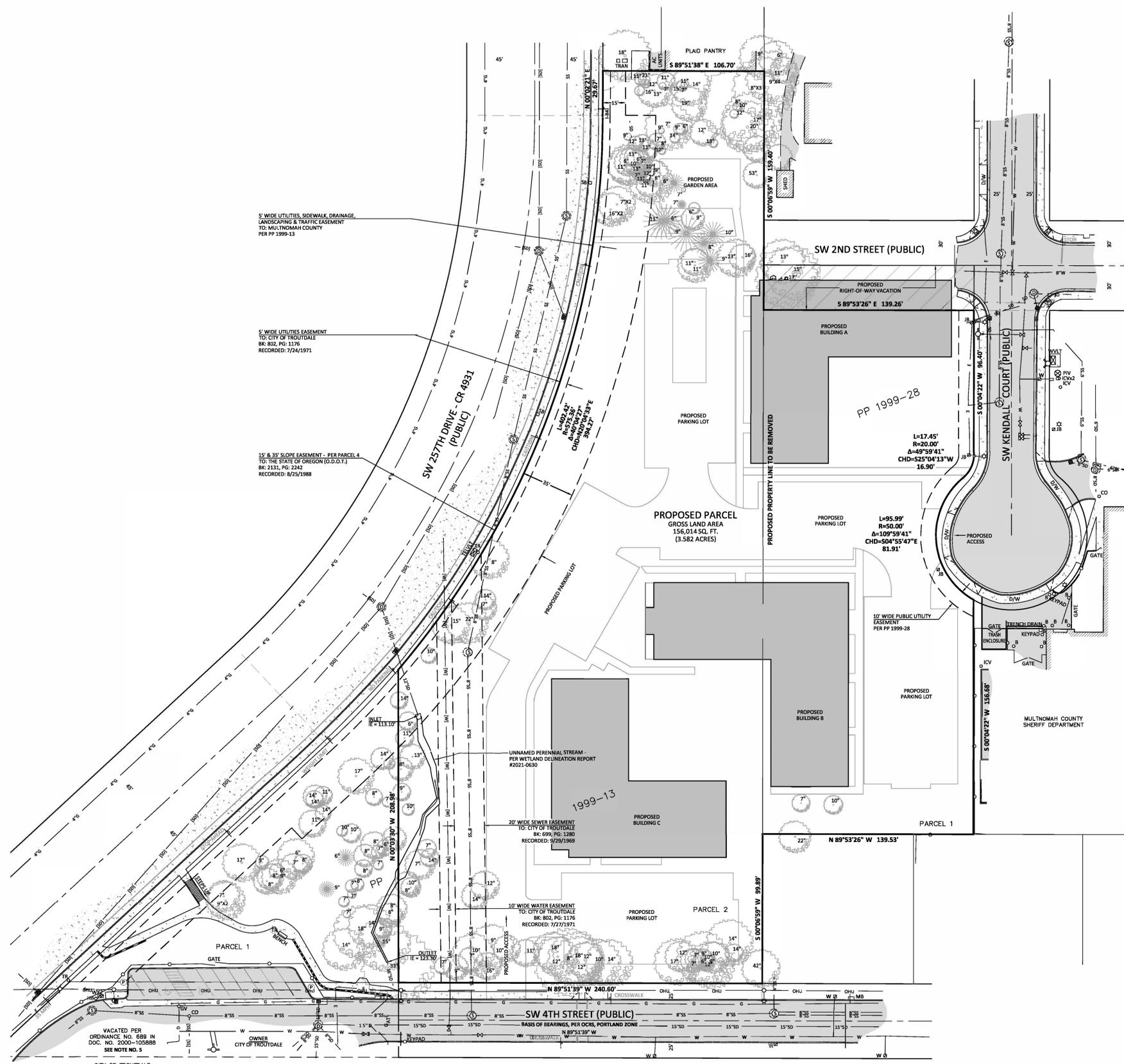


111 SW 5TH AVE., SUITE 2400
PORTLAND, OR 97204
P: 503.227.3251
F: 503.224.4681
www.kpff.com

DATE: 3 MAY 2022

CONTOUR INTERVAL: NA

SHEET NO. 1 OF 1



5' WIDE UTILITIES, SIDEWALK, DRAINAGE, LANDSCAPING & TRAFFIC EASEMENT TO: MULTNOMAH COUNTY PER PP 1899-13

5' WIDE UTILITIES EASEMENT TO: CITY OF TROUTDALE BK: 802, PG: 1176 RECORDED: 7/24/1971

15' & 35' SLOPE EASEMENT - PER PARCEL 4 TO: THE STATE OF OREGON (D.D.O.T.) BK: 2131, PG: 1242 RECORDED: 8/25/1988

UNNAMED PERENNIAL STREAM - PER WETLAND DELINEATION REPORT #2021-0630

20' WIDE SEWER EASEMENT TO: CITY OF TROUTDALE BK: 699, PG: 1280 RECORDED: 9/29/1969

10' WIDE WATER EASEMENT TO: CITY OF TROUTDALE BK: 802, PG: 1176 RECORDED: 7/27/1971

VACATED PER ORDINANCE NO. 689 IN DOC. NO. 2000-105888 SEE NOTE NO. 5

CITY OF TROUTDALE PUBLIC WORKS

REV.	DATE	BY	DESCRIPTION



111 SW 5TH AVE., SUITE 2400
PORTLAND, OR 97204
P: 503.227.3251
F: 503.224.4681
www.kpff.com

SURVEYED BY: BH
DRAWN BY: CMR
CHECKED BY: TTT
PROJECT NO.: 2100267
FILE: 2100267_PLOT_PLAN.dwg

PLOT PLAN SURVEY
SW 257TH DRIVE
CITY OF TROUTDALE
CITY OF TROUTDALE / MULTNOMAH COUNTY / OREGON



Date: August 24, 2022
From: Melissa Johnston, Associate Planner
To: Planning Commission
CC: Alex Lopez, Assistant Planner

Subject: Transportation System Plan, Draft Updates

Please continue to use the printed packets provided for the June 27th Transportation System Plan (TSP) workshop. The packet is also reposted online on the Planning Commission meeting page for August 24th at: <https://www.troutdaleoregon.gov/bc-pc/page/planning-commission-special-meeting-12>. Additional public comments that were not provided at June 27th meeting are attached.

Timeline:

- **Introduction** | December 1st, 2021 & January 5th, 2022 CAC meetings
- **Tech Memo #1: Existing Conditions and Future Needs** | March 2nd CAC meeting
Guest presentation by Kittelson & Associates
- **Tech Memo #2: Alternatives Analysis** | May 4th CAC meeting
Guest presentation by Kittelson & Associates
- **Review of Draft TSP Updates** | June 1st CAC meeting
Guest presentation by Kittelson & Associates
- **Additional comments and debrief** | July 6th CAC meeting
- **Planning Commission Briefing** | July 27th
- **City Council Briefing** | August 23rd
- **Planning Commission Hearing** | August 24th
Guest presentation by Kittelson & Associates
- **City Council Hearings** | Anticipated in September/October
Guest presentation by Kittelson & Associates

ATTACHED:

1. Public Comment: Paul Wilcox
2. Public Comment: Will Knight
3. Public Comment: Multnomah County
4. Public Comment Response

LINKS TO REFERENCED PLANS:

5. 2020-2040 [Town Center Plan, Chapter 2.4](#)
6. 2014 [Transportation System Plan](#)

From: [Paul Wilcox](#)
To: [Melissa Johnston](#)
Cc: [Alex Lopez](#); [Paul Wilcox](#)
Subject: TSP comments
Date: Thursday, July 28, 2022 3:24:42 PM

Hi Melissa,

I've taken a much more detailed look at the TSP, and have some questions and corrections.

First question is I don't find any reference to the recommendation for Sandy Ave. in a letter/number listing, just in the Kittelson discussion.

Other questions:

1. Estimated cost of Second St. bridge and connecting trails? I think Travis might be updating that.
2. Page 1-6. P-29 is red-lined thru. Wouldn't that be the Confluence riverfront trail?
3. Page 3-6. Why roadside signs instead of, or in addition to, sharrows?
4. Page 4-4. P-7 is red-lined thru. Why? Sidewalk project on Hensley.

Corrections:

1. Page 2-9. Should it read City Council instead of City County?
2. Page 3-8. Glen should be Glenn, as in Glenn Otto Park.

I saw someone caught a "than" vs "then". Good catch!

Paul

Sent from my iPad

6/10/22

Public Comment from Will Knight regarding the draft TSP file titled "final_tspchpt4_CAC Redlines".

To the planning commission and city council: Please do not consider this document to be fully shaped by and agreeable to the CAC under its role of citizen involvement.

I find it interesting that the file is titled "final_tspchpt4_CAC Redlines" as nothing that was discussed in the latest CAC meetings as requested changes or additional comments are included in this document. There are numerous issues that were specifically brought into the discussions by CAC members that have not been addressed. Information regarding Sandy avenue Geotech was finally released to the CAC the day prior to the June meeting. Issues that CAC members brought to the table at the June meeting have apparently been completely ignored. To title this document "final_tspchpt4_CAC Redlines" would suggest that the CAC had a large hand in crafting this document. I am not seeing that the CAC had much if any actual participation in this process. In addition, the CAC will not have the opportunity to pass along a formal recommendation to the council. That being said, I am inclined to say that the CAC, were given the role as spectators in this process and not as full participants.

My specific concerns regarding Sandy Avenue:

Since the days of the Town Center Committee, a false narrative has been advanced that Sandy Ave is in immediate danger of erosion and that the city had to do something quickly. As a result, an agenda to close Sandy Ave to one way traffic has been pushed by the planning department. According to the Geotechnical report titled "Geotechnical; Engineering Report SE 2nd street and SE Sandy Ave Stability Assessments Troutdale", dated 4/13/2018 by Shannon & Wilson INC, this couldn't be further from the truth. Shannon & Wilson's official report says that they cannot tell when exactly the activity happened, however it definitely happened prior to 2001. Shannon & Williams gave no indication that the ground is currently active. As of 6/10/2022 we have data to show that the ground has not been active for over 22 years!! Please also understand that the area to watch for ground activity encompasses *only 35 feet* of the entire quarter mile stretch of Sandy Ave that is being proposed for closure.

Furthermore, Shannon & Williams give their top 3 professional recommendations regarding what to do going forward:

Recommended action: Action 1 -Monitoring the slope for activity

Possible Mitigation Alternatives if issues happen to arise in the future:

Option 1 -Install Drainage within the Upper Slope

Option 2 -Install Reinforced Wall Structure

Please note that nowhere in this document do Shannon & Williams suggest closing the road to one way traffic. It is obvious to me that the truth has been stretched regarding the immediacy of any potential slope activity. The planning department has not followed the professional recommendations and have installed their own ideas into the Town Center Plan and now the TSP draft. I believe that advancing the agenda of closing Sandy Ave solely based upon the false narrative of immediate erosion concerns is both erroneous and deceitful.

The CAC had made at least 3 separate requests to have access to the Shannon & Wilson Geotech report as part of the ongoing discussions and were not provided the document. As a member of the Town Center Committee, I had asked for a copy of the report in 2021 when it was originally introduced. I finally received the report, on 5/31/22, after I made an additional specific request from the planning department. This information is not new to the city as the date of the geotechnical report is 4/13/2018, prior to any efforts made on the Town Center Plan or current draft TSP

My concerns regarding the Springwater Trail in Troutdale:

I would like to remind us all about the meeting held by Metro on February 22, 2017, regarding the Springwater Trail coming into Troutdale. Metro was asking for input regarding their plans to extend the Springwater Trail through from Depot park in Downtown Troutdale, up Sandy Ave and along Troutdale Road to Mt Hood CC. During that meeting over 200 residents showed overwhelming opposition to connecting Troutdale with the Springwater Trail. Much of the discussion centered around safety and livability concerns, especially transient and homeless camps, and crime. *Please keep in mind that the Sandy Avenue section of the Springwater Trail follows a path that goes behind Troutdale elementary school and ends right at the school playground.*

<https://www.oregonmetro.gov/news/troutdale-residents-help-shape-walking-cycling-trail>

The public was SO OPPOSED to the idea that metro immediately stopped their plans to build the trail through town.

<https://www.oregonmetro.gov/news/hearing-residents-metro-suspends-troutdale-trail>

My question is whether public sentiment has changed regarding this connection? I do not believe the sentiment has changed and the city has not done adequate outreach to poll the residents of Troutdale regarding the issue.

Therefore, if the city has the historical backdrop and outcome of the 2017 meeting and no new data to suggest the citizens now support the Springwater Trail in downtown; Then why is the city moving forward with the exact trail plans that metro walked away from after citing serious safety concerns from Troutdale residents?

“This trail that we’re talking about tonight will not be crammed down the citizens of Troutdale,” Ray Young said. “We will get to decide.”

Transportation Division - Planning & Development

TO Chris Damgen, City of Troutdale
Melissa Johnston, AICP, City of Troutdale

CC Jon P. Henrichsen, Multnomah County Transportation Division Director, County Engineer

FROM Eve Nilenders, Transportation Planning Specialist, Multnomah County
Jessica Berry, Transportation Planning and Development Manager, Multnomah County

DATE June 7, 2022

RE: Troutdale TSP - Draft Chapter 4 Comments

Thank you for giving us the opportunity to review and provide comments on proposed changes to Chapter 4 of Troutdale's Transportation System Plan (TSP). As previously noted, Multnomah County (County) and the City of Troutdale (City) have a long history of working together due to the overlapping nature of our jurisdictions and authorities. We reiterate some of the same comments and concerns provided previously, along with specific comments on the draft Chapter 4.

General Comments

Authority:

The TSP is a City policy document, and it is within the City's purview to identify projects on roads within the City. At the same time, there are several County roads that are arterials and collectors within the City; as such, the County is responsible for designing, constructing, and maintaining these roads. Multnomah County is also responsible for the health and safety of the traveling public on these roads. The County will consider including City road projects in our Capital Improvement Plan if the County is confident that doing so will align with the County's goals for safety in the right of way. Be advised that some proposed projects in the City's TSP do not currently conform to the County's standards for design based on the functional classification of the road; consequently, it is unlikely the County will include them in the County's Capital Improvement Plan without modifications and additional analyses performed by the City. See our comments below on AASHTO requirements in our Design and Construction Manual.

Financial Capability:

As noted in earlier communications, the County has limited funds available for capital projects. In earlier communications, the County requested that the City develop cost estimates for improvements and clearly identify which projects will be included in the City's Capital Improvement Plan to collect SDCs. The tables in Chapter 4 identify which projects the City intends to include in its CIP and notes where the City is willing to contribute 10% of the project funding. How was this contribution percentage determined? What is the City's expectation for identifying additional funds to construct the project? How were the cost estimates in the table

Transportation Division - Planning & Development

determined? Please be advised that not all projects in the City's TSP can be constructed and maintained by Multnomah County.

Multnomah County Design and Construction

Multnomah County follows AASHTO standards for roadway design. In addition, according to the Local Agency Guidelines (LAG) for Certified Local Public Agencies Manual, "The American Association of State Highway and Transportation Officials (AASHTO) guidelines have been adopted as the design standard for FHWA funded projects on local national highway system (NHS) routes and projects on the LPA's transportation system. Design standards for projects on the state highway system must conform to the requirements detailed in ODOT's current Highway Design Manual, other ODOT Manuals, and ODOT Technical Bulletins and Directives." Please also note that the County has not historically designed, constructed, or maintained off-street trails.

Equity and Environmental Justice in Transportation

Multnomah County is committed to applying a racial equity lens to our own policies and programs. In reviewing the edits to Chapter 4, we observed that the only mention of equity or environmental justice remains the final, unedited paragraph of Chapter 4, which is largely disconnected from the body of the chapter. The City may wish to consider expanding upon or making more explicit how its commitment to historically underserved and transportation-disadvantaged populations is reflected in the TSP priorities, given the incorporation of the Town Center Plan's visions and goals.

Specific Comments on TSP Chapter edits

1. Page 4-2: Why is the reference changing from the 2035 RTP to the 2010 RTP? This is confusing.
2. Page 4-5, Table 4-1: Project P46 is a Pedestrian/Bicycle Bridge over 257th Drive. Be advised that the County has concerns about constructing or allowing the construction of a facility over 257th Drive which serves as a Regional Emergency Transportation Route, Major Arterial, and Freight Route. We are also not convinced that the project cost will only be \$1.2M. We are also not clear where the City intends to find the remaining 90% of the project cost.
3. Page 4-6, Table 4-1: Table footnotes indicate "projects shown in gray are under the jurisdiction of other agencies". This update adds several projects that are under the jurisdiction of Multnomah County and are not shown in gray. **Prior to completion of this plan, please schedule a time with the County to review which projects are County projects vs. City projects.** For instance, many new projects on Historic Columbia River Highways are not shown in gray. Is this an oversight or does the City intend to seek a transfer?
4. Page 4-6, Table 4-1: Table footnotes indicate "...cost figures shown represent only the City's estimated contribution". Please clarify how the City's contribution was calculated.
5. Page 4-11, Table 4-3: Project B2 includes the statement, "Make Buxton Road an experimental street for electric bicycles, scooters, micro-transit, and golf carts". Be advised that Buxton Road is a County maintained Major Collector that carries +6,000 cars per day (2013 County Traffic Counts). The County has suggested that the City consider using a parallel local route for the experimental street. Vehicle diversion onto neighboring roads is likely to occur. This designation is not consistent with the County Design and Construction Manual.

Transportation Division - Planning & Development

6. Page 4-13 through 4-18: The Transit section does not include any new or innovative transit solutions, even though the region has done a fair bit of work (in particular, on Enhanced Transit Concepts) since the last TSP update. While these particular solutions may not be the most relevant for Troutdale, not addressing this seems like a missed opportunity. Among other things, the City might consider looking at the TriMet Pedestrian Plan, which analyzed and ranked pedestrian projects within each jurisdiction based on their potential to improve access to transit.
7. Page 4-32: the first paragraph under the section Motor Vehicle Master Plan states that “As a result [of Metro modeling efforts], a few notable projects from the 2005 TSP have been removed from this latest TSP update...” Is this new language? It isn’t redlined. If it is from the 2015 TSP, it should be removed - otherwise is not an accurate statement about this TSP version.
8. Page 4-33: Project M19 includes constructing an extension of Kibling Street over the railroad to the Confluence site. Be advised that the County would consider this a local street and would not include this in our Capital Plan.
9. Page 4-37: the Freight section states, “This update incorporates the conclusions and recommendations of the 2012 East Metro Connections Plan.” This is likely language from the 2015 update and should be removed.



Date: August 24, 2022
From: Melissa Johnston, Associate Planner
To: Planning Commission
CC: Alex Lopez, Assistant Planner

Public Comment Response

- Estimated cost of Second St. bridge and connecting trails?
 - Cost estimated in TSP will be changed to match the amount adopted in the CIP, \$2.8 million.
- Page 1-6. P-29 is red-lined thru. Wouldn't that be the Confluence riverfront trail?
 - We will keep the strike-through for P-29 and will add two projects: Harlow/levee trail and Sandy Riverfront Trail.
- Page 3-6. Why roadside signs instead of, or in addition to, sharrows?
 - Can discuss further at the meeting.
- Page 4-4. P-7 is red-lined thru. Why? Sidewalk project on Hensley.
 - This project is complete
- Corrections:
- Page 2-9. Should it read City Council instead of City County?
 - Thanks, we will correct.
- 2. Page 3-8. Glen should be Glenn, as in Glenn Otto Park.
 - Thanks, we will correct.
- Project proposal to close a lane of Sandy Avenue for a Bike/Pedestrian path has been removed from the TSP.
- Project proposal to make Buxton an experimental street for golf carts, e-scooters, micro transit has been removed from the TSP.